

AGENDA
REGULAR MEETING OF THE HOLTVILLE CITY COUNCIL
121 WEST FIFTH STREET, HOLTVILLE, CALIFORNIA

October 26, 2015

OPEN SESSION

6:00 PM

Ginger Ward, Mayor
Jim Predmore, Mayor Pro Tem
Mike Goodsell, Council Member
David Bradshaw, Council Member
Richard Layton, Council Member

Steve Walker, City Attorney
Denise Garcia, Interim City Clerk
Nick Wells, City Manager
George Morris, City Treasurer

THIS IS A PUBLIC MEETING

If there is an item on the agenda on which you wish to be heard, please come forward to the microphone at the time the item is being addressed. State your name for the record. All other items can be addressed during the Public Comments portion of the agenda. The Mayor reserves the right to place a time limit on each person asking to be heard.

CITY COUNCIL

CLOSED SESSION 5:30 PM

CONFERENCE WITH LABOR NEGOTIATORS:

(Government Code Section 54957(b)(1))

Agency Negotiator: City Manager/City Attorney
Classified Employees

PUBLIC EMPLOYMENT:

(Government Code Section 54957.6)

City Manager Evaluation
Evaluation Criteria

CONFERENCE WITH LEGAL COUNSEL – PENDING LITIGATION:

(Government Code Section 54956.9(a))

City of Holtville vs. Black Dog Farms
City of Holtville vs. Willowbend Mobile Home Park
Charles Simpson vs. City of Holtville

CITY COUNCIL: Meeting Called To Order

PLEDGE OF ALLEGIANCE:

INVOCATION:

CITY CLERK RE: Verification of Posting of the Agenda

EXECUTIVE SESSION ANNOUNCEMENTS: Steve Walker, City Attorney

PUBLIC COMMENTS: This is time for the public to address the City Council on any item not appearing on the agenda that is within the subject matter jurisdiction of the City Council. The Mayor reserves the right to limit the speaker's time. The Mayor will recognize you and when you come to the microphone, please state your name and address for the record. You are not allowed to make personal attacks on individuals or make comments which are slanderous or which may invade an individual's personal privacy.

CONSENT AGENDA:

The items on the Consent Agenda are to be approved without comment. Should any Council Member or member of the public wish to discuss any item, they may request that the item be removed from the Consent Agenda and placed on the New Business Agenda.

1. Approval of the Minutes from the Meeting of October 12, 2015.
2. Current Demands 35213 to 35269.

REPORTS OF OFFICERS, COMMISSIONS, COMMITTEES AND COMMUNIQUES:

UNFINISHED BUSINESS:

None

NEW BUSINESS:

3. **Discussion/Related Action** to adopt **RESOLUTION #15-32** approving the Sewer System Management Plan Update as mandated by the State Water Resources Control Board – Jack Holt, City Engineer
4. **Discussion/Related Action** to adopt **RESOLUTION #15-33** Authorizing the Investment of City Funds in Certificates of Deposit and other Federally Insured Instruments – Nick Wells, City Manager
5. **Discussion/Related Action** to adopt **RESOLUTION #15-34** Waiving the Application of Water Enterprise Impact Fees to a New Residential Connection at 2076 Mets Road Outside of the City Limits – Nick Wells, City Manager
6. **Discussion/Related Action** Regarding the Appointment of a Permanent City Clerk – Nick Wells, City Manager

INFORMATION ONLY:

7. **Discussion Only** Regarding a Draft Complete Streets Plan as Presented by Alta Planning and Design – Nick Wells, City Manager
8. City Manager Report – Nick Wells
 - a. Fire Chief – Alex Silva
 - b. Water/Wastewater Supervisor – Frank Cornejo
 - c. Public Works Supervisor – Alex Chavez
9. Items for Future Meetings:

ADJOURNMENT:

I, Denise Garcia, Interim City Clerk of the City of Holtville, California, **DO HEREBY CERTIFY** that the foregoing agenda was duly posted at Holtville City Hall on October 23, 2015.

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**THE MINUTES OF THE REGULAR MEETING OF
THE HOLTVILLE CITY COUNCIL**

October 12, 2015

MEETING DATE:	10-20-15
APPROVED FOR AGENDA	
CITY MANAGER	
FINANCE MANAGER	
CITY ATTORNEY	

The regular meeting of the Holtville City Council was held on Monday, October 12, 2015 at 5:30 p.m. in the Civic Center. Council Members present were Mike Goodsell, Ginger Ward, Richard Layton, David Bradshaw and Jim Predmore. Staff members present were Nick Wells, Denise Garcia and Manuel DeLeon. City Attorney Steve Walker, City Planner Justina Arce, City Treasurer George Morris were also present. City Engineer Jack Holt was absent.

CITY COUNCIL CLOSED SESSION MEETING CALLED TO ORDER:

Mayor Ginger Ward called the meeting to order at 5:32 p.m.

CITY COUNCIL & SUCCESSOR AGENCY OPEN SESSION MEETING CALLED TO ORDER:

Mayor Ginger Ward called the meeting to order at 6:07 p.m.

PLEDGE OF ALLEGIANCE:

The Pledge of Allegiance was led by Mayor Ginger Ward.

INVOCATION:

Pastor Richard Moore, Trinity Baptist Church

CITY CLERK RE: VERIFICATION OF POSTING OF AGENDA:

The Interim City Clerk verified that the amended agenda was duly posted on October 8, 2015.

EXECUTIVE SESSION ANNOUNCEMENTS:

None

SWEARING IN OF CITY TREASURER:

George Morris was sworn in as the Appointed City Treasurer to serve the remaining term of the vacancy left by Elwood "Pete" Mellinger.

PUBLIC EMPLOYEE PERFORMANCE EVALUATION/DISCIPLINE/DISMISSAL/RELEASE:

(Government Code Section 54957(b 1))

TITLE: City Clerk

No Reportable Action Taken.

PUBLIC EMPLOYMENT:

(Government Code Section 54957.6)

City Manager Evaluation

Evaluation Criteria

No Reportable Action Taken.

CONFERENCE WITH LEGAL COUNSEL – PENDING LITIGATION:

(Government Code Section 54956.9(a))

City of Holtville vs. Black Dog Farms

City of Holtville vs. Willowbend Mobile Home Park

No Reportable Action Taken.

PUBLIC COMMENTS:

None

CONSENT AGENDA:

- 1. Approval of the Regular Minutes of the City Council Meeting of September 28, 2015.**
- 2. Current Demands #35150 To #35212.**

A motion was made by Mr. Layton and seconded by Mr. Bradshaw to approve items 1 and 2 of the Consent Agenda. The motion carried in the form of a roll call vote.

AYES: Ward, Goodsell, Predmore, Layton, Bradshaw
NOES: None
ABSENT: None
ABSTAIN: None

REPORTS OF OFFICERS, COMMISSIONS, COMMITTEES AND COMMUNIQUES:

Manuel Deleon reported that he attended the Walk to School event and there were several children that participated. He informed Council that the Trans Continental Convoy would be passing through Holtville. Council was also advised that Deputies patrolling Holtville would be wearing Holtville Patches to represent their status as Holtville Deputies. Council was then introduced to Deputy Ayala who was responsible for designing the patch.

Richard Layton reported that he attended the Holtville Chamber of Commerce 24 Carrot Award Dinner.

Jim Predmore reported that attended the Holtville Chamber of Commerce 24 Carrot Award Dinner held at the Swiss Club, where Dora DePaoli was given the award. Jim also informed Council that he attended the Walk to School event and a SCAG Meeting last Thursday.

Ginger reported that she attended the Holtville Chamber of Commerce 24 Carrot Award Dinner and the Walk to School event. She informed Council that she would be attending the Holtville Farmer's Market this Thursday as well as the passing of the Convoy.

Mike Goodsell reported that he attended the Holtville Chamber of Commerce 24 Carrot Award Dinner and the funeral of Jeanne Widmann. He also announced that the Holtville High School Football team finally won a game.

Nick Wells reported that he attended EOP Training, the Chamber Dinner, the final day of the Walk to School Event, and is anticipating the Grand Opening of the Auto Zone. He informed Council that the Solar Ponds project was being scaled down in size, but still moving along.

UNFINISHED BUSINESS:

None

NEW BUSINESS:

- 3. Discussion/Related Action to adopt RESOLUTION #15-31 Authorizing the City Manager to Execute an Agreement with the State Board of Equalization for Collection of Utility User Tax on Prepaid Phone Cards – Nick Wells, City Manager**

A motion was made by Mr. Predmore and seconded by Mr. Goodsell to adopt Resolution #15-31 authorizing the City Manager to execute an agreement with the State Board of Equalization for collection of Utility User Tax on prepaid phone cards. All members present were in favor and the motion carried in the form of a roll call vote.

AYES: Ward, Goodsell, Predmore, Layton, Bradshaw
NOES: None
ABSENT: None
ABSTAIN: None

INFORMATION ONLY:

4. Information Only Regarding the Army Transcontinental Convoy Passing through Holtville on Friday, October 16, 2015 – Nick Wells, City Manager

The City Manager informed Council that the Convoy would not be stopping in Holtville, but that the City of El Centro has invited Council to attend their dinner that same evening where the Convoy would stop.

5. City Manager Report – Nick Wells

- a. **Police Chief – Manuel DeLeon**
- b. **Water/Wastewater Supervisor – Frank Cornejo**
- c. **Public Works Supervisor – Alex Chavez**

Written reports were provided by the following: City Manager, Water/Wastewater Supervisor, Police Chief, and Public Works Foreman.

6. Items for Future Meetings

None

There being no further business to come before Council, Mayor Ward adjourned the meeting at 6:38 p.m.

Virginia Ward, Mayor

Denise Garcia, Interim City Clerk

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CITY OF HOLTVILLE Monthly Check Register

MEETING DATE: 10-26-15
 Page: 1
 APPROVED FOR AGENDA
 CITY MANAGER _____
 FINANCE MANAGER _____
 CITY ATTORNEY _____

Date : 10/22/2015 9:58:33 AM
User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

Fund	Transaction	Due Date	Invoice Number	Description	PO Number	Amount
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Vendor : 8686 PRO-LAB						
10	69730	10/19/2015	SAMPLE 1	MOLD TESTING AT FIRE STATION	FD	55.00
Invoice Amount : 55.00		Discount Amount : 0.00		Check Amount :		55.00
Check Number : 35214 Check Date : 10/20/2015						
Vendor : 8686 PRO-LAB						
10	69731	10/19/2015	SAMPLE 2	MOLD TESTING AT FIRE STATION	FD	55.00
Invoice Amount : 55.00		Discount Amount : 0.00		Check Amount :		55.00
Check Number : 35215 Check Date : 10/20/2015						
Vendor : 8671 A1 JANITORIAL SUPPLY						
12	69649	10/14/2015	A1S15155	DRAIN SEWER OPENER	PW	346.23
Invoice Amount : 346.23		Discount Amount : 0.00		Check Amount :		346.23
Check Number : 35216 Check Date : 10/20/2015						
Vendor : 1065 ACME SAFETY & SUPPLY CO.						
10	69637	10/14/2015	98985-00	SIGNS; DECLAS; BARRICADE WITH LI	PW	214.92
Invoice Amount : 214.92		Discount Amount : 0.00		Check Amount :		214.92
Check Number : 35217 Check Date : 10/20/2015						
Vendor : 1452 AVAYA , INC.						
10	69661	10/14/2015	101886669	TELEPHONE EQUIPMENT (PD)	PD	69.77
Invoice Amount : 69.77		Discount Amount : 0.00		Check Amount :		69.77
Check Number : 35218 Check Date : 10/20/2015						
Vendor : 8166 BOUNDTREE						
10	69727	10/20/2015	81915964	MEDICAL SUPPLIES	FD	984.99
Invoice Amount : 984.99		Discount Amount : 0.00		Check Amount :		984.99
Check Number : 35219 Check Date : 10/20/2015						
Vendor : 1710 BRITHINEE ELECTRIC						
11	69651	10/14/2015	S104265	CONSTROL/FIELD SERVICE LABOR	OW	1,200.00
Invoice Amount : 1,200.00		Discount Amount : 0.00		Check Amount :		1,200.00
Check Number : 35220 Check Date : 10/20/2015						
Vendor : 8676 BRYANT REPORTING AGENCY						
10	69653	10/14/2015	1998	DEPOSITION OF DARRELL GORDON	City v. Black Dog	509.25
10	69668	10/14/2015	1996	DEPOSITION OF MEHDI RASHTI		729.75
10	69669	10/14/2015	1971	DEPOSITION OF JAMES MC MULLEN		588.00
10	69670	10/14/2015	2006	DEPOSITION OF TIM VALINE (BLAC		837.50
Invoice Amount : 2,664.50		Discount Amount : 0.00		Check Amount :		2,664.50
Check Number : 35221 Check Date : 10/20/2015						
Vendor : 7688 CINTAS FIRST AID & SAFETY						
10	69680	10/14/2015	5003755733	SERVICE FIRST AID BOX AT CITY HA	PW	76.78
Invoice Amount : 76.78		Discount Amount : 0.00		Check Amount :		76.78
Check Number : 35222 Check Date : 10/20/2015						
Vendor : 8119 CITY OF IMPERIAL						
10	69711	10/19/2015	1761	PLAN CHECK FOR AUGUST AND SEPT	Bldg. / Planning	7,922.81
Invoice Amount : 7,922.81		Discount Amount : 0.00		Check Amount :		7,922.81
Check Number : 35223 Check Date : 10/20/2015						
Vendor : 1907 COUNTY OF SAN DIEGO, RCS						
10	69728	10/20/2015	16HOLTFDN03	800 mhz RADIOS	FD/PW	733.33
11	69728	10/20/2015	16HOLTFDN03	800 mhz RADIOS		87.08
12	69728	10/20/2015	16HOLTFDN03	800 mhz RADIOS		87.09
Invoice Amount : 907.50		Discount Amount : 0.00		Check Amount :		907.50
Check Number : 35224 Check Date : 10/20/2015						
Vendor : 7932 CR&R INCORPORATED						

CITY OF HOLTVILLE
Monthly Check Register

Date : 10/22/2015 9:58:33 AM

User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

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10	69671	10/14/2015	SEPT. 2015	TRASH SERVICE SEPTEMBER 2015		
13	69671	10/14/2015	SEPT. 2015	TRASH SERVICE SEPTEMBER 2015	Trash	(3,219.24)
Invoice Amount		: 13,771.22	Discount Amount	: 0.00	Check Amount	: 13,771.22
Check Number		: 35225	Check Date		: 10/20/2015	
Vendor : 1435 DEL SOL MARKET						
10	69723	10/19/2015	2739	GATORADE FOR WATER BREAK	PW	29.69
Invoice Amount		: 29.69	Discount Amount	: 0.00	Check Amount	: 29.69
Check Number		: 35226	Check Date		: 10/20/2015	
Vendor : 1845 DEPT OF JUSTICE						
10	69724	10/19/2015	124671	FINGERPRINTS FD	FD	32.00
Invoice Amount		: 32.00	Discount Amount	: 0.00	Check Amount	: 32.00
Check Number		: 35227	Check Date		: 10/20/2015	
Vendor : 1540 DRAGON'S EXTERMINATOR						
10	69696	10/14/2015	137434	PEST CONTROL FOR CITY BLDGS.	PW	200.00
Invoice Amount		: 200.00	Discount Amount	: 0.00	Check Amount	: 200.00
Check Number		: 35228	Check Date		: 10/20/2015	
Vendor : 1010 ELMS EQUIPMENT RENTALS						
10	69722	10/19/2015	21064460001	SERVICE CHAIN SAW FD	FD	48.75
Invoice Amount		: 48.75	Discount Amount	: 0.00	Check Amount	: 48.75
Check Number		: 35229	Check Date		: 10/20/2015	
Vendor : 4572 EMPIRE SOUTHWEST						
12	69647	10/14/2015	EPWK0374262	REPLACE BLOCK HEATER	PW	1,077.39
Invoice Amount		: 1,077.39	Discount Amount	: 0.00	Check Amount	: 1,077.39
Check Number		: 35230	Check Date		: 10/20/2015	
Vendor : 1536 FERGUSON ENTERPRISES, INC. #8423						
11	69678	10/14/2015	2300483-1	LID WTR	PW	111.40
11	69679	10/14/2015	2300483	VALVE BOX; WATER LID		185.65
Invoice Amount		: 297.05	Discount Amount	: 0.00	Check Amount	: 297.05
Check Number		: 35231	Check Date		: 10/20/2015	
Vendor : 4662 FERNANDO RUIZ, INC.						
10	69628	10/13/2015	126392	SAFTEY TRAINING OCT. 2015		
11	69628	10/13/2015	126392	SAFTEY TRAINING OCT. 2015	PW	235.00
12	69628	10/13/2015	126392	SAFTEY TRAINING OCT. 2015		235.00
Invoice Amount		: 705.00	Discount Amount	: 0.00	Check Amount	: 705.00
Check Number		: 35232	Check Date		: 10/20/2015	
Vendor : 2227 GEORGE'S PIZZA						
10	69709	10/19/2015	784636	CITY MANAGER'S MONTHLY MEETING	Admin	60.01
11	69710	10/19/2015	784631	FOOD FOR PUBLIC WORKS AFTER WA	PW	58.04
Invoice Amount		: 118.05	Discount Amount	: 0.00	Check Amount	: 118.05
Check Number		: 35233	Check Date		: 10/20/2015	
Vendor : 8685 HARBOR FREIGHT TOOLS						
11	69672	10/14/2015	3143667	SUBMERSIBLE UTILITY; SHALLOW WE	PW	445.92
Invoice Amount		: 445.92	Discount Amount	: 0.00	Check Amount	: 445.92
Check Number		: 35234	Check Date		: 10/20/2015	
Vendor : 1471 HD SUPPLY WATERWORKS, LTD						
11	69632	10/13/2015	E548419	HYD BURY; STARGRIP RESTRAINT; GS		
11	69633	10/13/2015	E446385	FLG PIPE; GASKET; JONES STEM NUT	PW	856.63
11	69635	10/14/2015	E505531	3.5 QUANTUMSS CPLG, EPOXY		447.51
11	69652	10/14/2015	E446385-1	1 1/2 CPLGS		200.20
11	69684	10/14/2015	E516429	16" FORCE4 GUIDE BAR		154.45
11	69686	10/14/2015	E490909	CLAMP		321.31
						94.10

CITY OF HOLTVILLE
Monthly Check Register

Date : 10/22/2015 9:58:33 AM

User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

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11	69694	10/14/2015	E572616	4" ROMAC MACRO CPLG; HYD STEM		625.95
Invoice Amount		: 2,700.15	Discount Amount	: 0.00	Check Amount	: 2,700.15
Check Number		: 35235	Check Date	: 10/20/2015		
Vendor		: 1491 HELENA CHEMICAL CO				
10	69636	10/14/2015	228964633	ROUNDUP	PW	135.00
Invoice Amount		: 135.00	Discount Amount	: 0.00	Check Amount	: 135.00
Check Number		: 35236	Check Date	: 10/20/2015		
Vendor		: 1147 HOLTVILLE CHAMBER OF COMMERCE				
10	69659	10/14/2015	10/8/15	24 CARROTT GOLD AWARD DINNER	Council	175.00
Invoice Amount		: 175.00	Discount Amount	: 0.00	Check Amount	: 175.00
Check Number		: 35237	Check Date	: 10/20/2015		
Vendor		: 1638 HOME DEPOT/GECF				
10	69640	10/14/2015	32914	RECIP SAW; LASER LEVEL; CIRC SA	PW	366.28
10	69685	10/14/2015	7260032	TREE PRUNER; EDGER BLADES;		115.39
Invoice Amount		: 481.67	Discount Amount	: 0.00	Check Amount	: 481.67
Check Number		: 35238	Check Date	: 10/20/2015		
Vendor		: 1024 I.C. HEALTH DEPARTMENT				
11	69645	10/14/2015	17169	LAB ANALYSIS	PW	120.00
10	69725	10/19/2015	16569	PHYSICAL NEW EMPLOYEE FD	FD	425.44
Invoice Amount		: 545.44	Discount Amount	: 0.00	Check Amount	: 545.44
Check Number		: 35239	Check Date	: 10/20/2015		
Vendor		: 1150 I.V. HUMANE SOCIETY				
10	69674	10/14/2015	OCT. 2015	ANIMAL CARE EXP. FOR JULY; AUGUS	ACO	972.00
Invoice Amount		: 972.00	Discount Amount	: 0.00	Check Amount	: 972.00
Check Number		: 35240	Check Date	: 10/20/2015		
Vendor		: 1153 I.V. REGIONAL OCCUPATIONAL PROGRAM				
10	69712	10/19/2015	8699--00	FINGERPRINTING CHARGES FD	FD	25.00
Invoice Amount		: 25.00	Discount Amount	: 0.00	Check Amount	: 25.00
Check Number		: 35241	Check Date	: 10/20/2015		
Vendor		: 2297 IMPERIAL COUNTY SHERIFF'S DEPT.				
10	69665	10/14/2015	09302015-1	SHERIFF SERVICES FOR SEPTEMBER	Sheriff	81,931.06
Invoice Amount		: 81,931.06	Discount Amount	: 0.00	Check Amount	: 81,931.06
Check Number		: 35242	Check Date	: 10/20/2015		
Vendor		: 7772 IMPERIAL LANDFILL				
10	69693	10/14/2015	14101	LANDFIL FEES	ACO	54.80
Invoice Amount		: 54.80	Discount Amount	: 0.00	Check Amount	: 54.80
Check Number		: 35243	Check Date	: 10/20/2015		
Vendor		: 1027 IMPERIAL STORES				
11	69676	10/14/2015	562433	keys		4.90
11	69677	10/14/2015	562425	LEVEL		6.63
12	69677	10/14/2015	562425	LEVEL		6.64
11	69681	10/14/2015	561820	HOSE CLAMP		2.59
11	69682	10/14/2015	561810	WET DRY CEMENT; PVC PRIMER; ADAP		22.67
10	69688	10/14/2015	562164	3 BLADE TIMMER; LOCK COUPLER; T		29.44
10	69714	10/19/2015	561031	FILTERS		18.35
10	69715	10/19/2015	561621	POSTER BOARD; PICTURE HANGER S		7.44
10	69716	10/19/2015	561814	MOLD REMOVER SPRAY; CLEANING S		41.69
10	69717	10/19/2015	561798	CLOTHES SOAP; SOFTENER		22.87
Invoice Amount		: 163.22	Discount Amount	: 0.00	Check Amount	: 163.22
Check Number		: 35244	Check Date	: 10/20/2015		
Vendor		: 1757 IMPERIAL VALLEY ENVIRONMENTAL LAB				

CITY OF HOLTVILLE

Monthly Check Register

Date : 10/22/2015 9:58:33 AM

User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

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12	69630	10/13/2015	7702	LAB ANALYSIS		1,764.00
12	69642	10/14/2015	7715	LAB ANALYSIS	PW	198.00
Invoice Amount		: 1,962.00	Discount Amount	: 0.00	Check Amount	: 1,962.00
Check Number		: 35245	Check Date	: 10/20/2015		
Vendor : 7927 INTERNAL REVENUE SERVICE						
10	69708	10/19/2015	95-6000721	TAXES AND INTEREST DUE ON 941 &	Admin	11,197.05
Invoice Amount		: 11,197.05	Discount Amount	: 0.00	Check Amount	: 11,197.05
Check Number		: 35246	Check Date	: 10/20/2015		
Vendor : 1382 K-C WELDING & RENTAL, INC.						
11	69689	10/14/2015	B46270	WORK BOOTS FOR FRANK CORNEJO		100.00
12	69689	10/14/2015	B46270	WORK BOOTS FOR FRANK CORNEJO	PW	100.00
Invoice Amount		: 200.00	Discount Amount	: 0.00	Check Amount	: 200.00
Check Number		: 35247	Check Date	: 10/20/2015		
Vendor : 2278 LA BRUCHERIE IRRIGATION SUPPLY						
10	69675	10/14/2015	109717c	SPRINKLER REPAIR PARTS		46.03
10	69683	10/14/2015	108920c	SPRINKLERS AND REPAIR PARTS	PW	598.76
Invoice Amount		: 644.79	Discount Amount	: 0.00	Check Amount	: 644.79
Check Number		: 35248	Check Date	: 10/20/2015		
Vendor : 8341 LABOR COMPLIANCE CONSULTANTS OF SO CALIFORNIA						
22	69663	10/14/2015	239	LABOR COMPLIANCE	Alamo Trail	287.00
Invoice Amount		: 287.00	Discount Amount	: 0.00	Check Amount	: 287.00
Check Number		: 35249	Check Date	: 10/20/2015		
Vendor : 2233 LINCOLN EQUIPMENT						
10	69695	10/14/2015	SI277238	PULSAR PLUS BRIQUETES	PW	4,314.27
Invoice Amount		: 4,314.27	Discount Amount	: 0.00	Check Amount	: 4,314.27
Check Number		: 35250	Check Date	: 10/20/2015		
Vendor : 1095 MOSS, LEVY & HARTZHEIM (bh)						
10	69664	10/14/2015	6283	COMPLETION OF SCO STREET REPOR	Admin	1,000.00
Invoice Amount		: 1,000.00	Discount Amount	: 0.00	Check Amount	: 1,000.00
Check Number		: 35251	Check Date	: 10/20/2015		
Vendor : 1361 PHOENIX UNIFORM						
10	69720	10/19/2015	235386	UNIFORMS SEWING; PATCHES	PW	235.39
Invoice Amount		: 235.39	Discount Amount	: 0.00	Check Amount	: 235.39
Check Number		: 35252	Check Date	: 10/20/2015		
Vendor : 1405 PITNEY BOWES - PURCHASE POWER						
10	69660	10/14/2015	OCT. 2015	POSTAGE		154.50
11	69660	10/14/2015	OCT. 2015	POSTAGE	Admin	720.00
12	69660	10/14/2015	OCT. 2015	POSTAGE		720.00
Invoice Amount		: 1,594.50	Discount Amount	: 0.00	Check Amount	: 1,594.50
Check Number		: 35253	Check Date	: 10/20/2015		
Vendor : 1746 RDO EQUIPMENT CO.						
10	69691	10/14/2015	29004	REPAIRS TO LAWN MOWER	PW	740.57
Invoice Amount		: 740.57	Discount Amount	: 0.00	Check Amount	: 740.57
Check Number		: 35254	Check Date	: 10/20/2015		
Vendor : 1555 ROBERT S. NELSON AUTOMOTIVE						
10	69729	10/20/2015	11185	REPAIRS TO VEHICLE (FD)	FD	41.59
Invoice Amount		: 41.59	Discount Amount	: 0.00	Check Amount	: 41.59
Check Number		: 35255	Check Date	: 10/20/2015		
Vendor : 1043 SELLERS PETROLEUM						
10	69690	10/14/2015	CL77833	FUEL (PW)		1,115.28
11	69690	10/14/2015	CL77833	FUEL (PW)	PW	468.08

CITY OF HOLTVILLE

Monthly Check Register

Date : 10/22/2015 9:58:33 AM

User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

Fund	Transaction	Due Date	Invoice Number	Description	PO Number	Amount
12	69690	10/14/2015	CL77833	FUEL (PW)		457.20
10	69721	10/19/2015	CL77832	FUEL (FD)		FD 782.30
Invoice Amount		: 2,822.86	Discount Amount	: 0.00	Check Amount	: 2,822.86
Check Number		: 35256	Check Date	: 10/20/2015		
Vendor		: 1109 SPRINT				
11	69648	10/14/2015	984297987-087	DATE LINE FOR WATER PLANT		PW 24.28
Invoice Amount		: 24.28	Discount Amount	: 0.00	Check Amount	: 24.28
Check Number		: 35257	Check Date	: 10/20/2015		
Vendor		: 2318 SUNBELT RENTALS				
11	69650	10/14/2015	55349655	SUBMERSIBLE DEWATERING PUMP		PW 496.80
Invoice Amount		: 496.80	Discount Amount	: 0.00	Check Amount	: 496.80
Check Number		: 35258	Check Date	: 10/20/2015		
Vendor		: 2217 SUPERIOR READY MIX CONCRETE				
10	69638	10/14/2015	735503	CONCRETE FOR 6TH AND MELON		282.78
10	69639	10/14/2015	736217	CONCRETE FOR 6TH AND MELON		PW 282.78
10	69687	10/14/2015	730874	CEMENT FOR 5TH STREET AND ORAN		275.28
10	69692	10/14/2015	736239	EXPANSION JOINT, MARKING WAND		110.72
Invoice Amount		: 951.56	Discount Amount	: 0.00	Check Amount	: 951.56
Check Number		: 35259	Check Date	: 10/20/2015		
Vendor		: 1775 SWRCB				
12	69631	10/13/2015	EW-1000779	WATER SYSTEM ENFORCEMENT FEES 7		PW 612.00
Invoice Amount		: 612.00	Discount Amount	: 0.00	Check Amount	: 612.00
Check Number		: 35260	Check Date	: 10/20/2015		
Vendor		: 2083 THATCHER CO.				
11	69629	10/13/2015	237390	ALUM SULFATE		PW 4,489.95
11	69646	10/14/2015	237272	T CHLOR		4,798.57
Invoice Amount		: 9,288.52	Discount Amount	: 0.00	Check Amount	: 9,288.52
Check Number		: 35261	Check Date	: 10/20/2015		
Vendor		: 1049 UNDERGROUND SERVICE ALERT				
10	69641	10/14/2015	920150320	DIG ALERTS		PW 21.00
Invoice Amount		: 21.00	Discount Amount	: 0.00	Check Amount	: 21.00
Check Number		: 35262	Check Date	: 10/20/2015		
Vendor		: 1260 USA BLUEBOOK				
12	69643	10/14/2015	751007	LEVEL TRANSMITTER		PW 738.08
Invoice Amount		: 738.08	Discount Amount	: 0.00	Check Amount	: 738.08
Check Number		: 35263	Check Date	: 10/20/2015		
Vendor		: 2524 VALLEY TESTING				
10	69713	10/19/2015	26010	DRUG TESTING NEW EMPLOYEE		FD 42.00
Invoice Amount		: 42.00	Discount Amount	: 0.00	Check Amount	: 42.00
Check Number		: 35264	Check Date	: 10/20/2015		
Vendor		: 8362 VERITEXT				
10	69654	10/14/2015	SD2420807	DEPOSITION OF THOMAS JOSEPH	City v. Black Dog	1,007.90
10	69655	10/14/2015	SD2419599	DEPOSITION OF SIMON WONG		606.40
10	69656	10/14/2015	SD2419613	DEPOSITION OF ANDREW MILLER		475.95
10	69657	10/14/2015	SD2406173	DEPOSITION OF OSCAR SEVILLA		804.25
Invoice Amount		: 2,894.50	Discount Amount	: 0.00	Check Amount	: 2,894.50
Check Number		: 35265	Check Date	: 10/20/2015		
Vendor		: 1231 WALKER & DRISKILL				
10	69658	10/14/2015	6894	ATTORNEY FEES CITY OF HOLTVILLE	City v. Black Dog	31,251.85
10	69667	10/14/2015	6899	ATTORNEY FEES	Admin	735.00
Invoice Amount		: 31,986.85	Discount Amount	: 0.00	Check Amount	: 31,986.85

CITY OF HOLTVILLE Monthly Check Register

Date : 10/22/2015 9:58:33 AM

User Name : Lee Ann

Calendar Month/Year : <ALL> <ALL>

Fund	Transaction	Due Date	Invoice Number	Description	PO Number	Amount
Check Number : 35266 Check Date : 10/20/2015						
Vendor : 1942 WASTE CORP						
12	69644	10/14/2015	IN201880	CONN ROD ASSY PE A W/BRZ INSERT	PW	1,473.71
Invoice Amount : 1,473.71		Discount Amount : 0.00		Check Amount :		1,473.71
Check Number : 35267 Check Date : 10/20/2015						
Vendor : 8193 WESTAIR GASES & EQUIPMENT						
10	69673	10/14/2015	39055	ACETYLENE	PW	369.82
Invoice Amount : 369.82		Discount Amount : 0.00		Check Amount :		369.82
Check Number : 35268 Check Date : 10/20/2015						
Vendor : 1058 XEROX CORPORATION						
10	69627	10/13/2015	137090166	COPIER (FD)	FD	108.00
10	69662	10/14/2015	81570448	COPIER LEASE AND USAGE		270.51
11	69662	10/14/2015	81570448	COPIER LEASE AND USAGE		95.51
12	69662	10/14/2015	81570448	COPIER LEASE AND USAGE	Admin.	95.53
10	69718	10/19/2015	137090166;81570	NEW COPIER ORDER FEE; OCTOBER		224.45
10	69719	10/19/2015	80725759;804524	COPIER LEASE AND USAGE FOR AUG	FD	139.18
Invoice Amount : 933.18		Discount Amount : 0.00		Check Amount :		933.18
Check Number : 35269 Check Date : 10/21/2015						
Vendor : 8011 NICHOLAS WELLS						
10	69763	10/21/2015	15-01	LUNCH FOR EOC TRAINING / MEETING		99.00
10	69764	10/21/2015	OCT. 2015	MILEAGE FOR OCTOBER 2015	Admin.	145.48
Invoice Amount : 244.48		Discount Amount : 0.00		Check Amount :		244.48

Total Number of Vendors	56	0.00
Total Number of Checks Printed	57	
Total Number of Voided Checks	0	
Largest Check Amount	81,931.06	
Total for all Checks Printed	193,522.71	
Total for Voided Checks	0.00	
Net Amount of Checks Printed	193,522.71	

Summary

Fund	Amount
10 GENERAL FUND	151,961.16
11 WATER	16,373.22
12 SEWER	7,910.87
13 TRASH	16,990.46
22 LOCAL TRANSPORTATION FUND - TDA	287.00

3

MEETING DATE:	10-26-15
APPROVED FOR AGENDA	<i>[Signature]</i>
CITY MANAGER	_____
FINANCE MANAGER	_____
CITY ATTORNEY	_____

City of Holtville

REPORT TO CITY COUNCIL

DATE ISSUED: October 26, 2015

FROM: James G. "Jack" Holt, P.E., City Engineer

SUBJECT: Resolution No. 15-32
A Resolution of the Holtville City Council Adopting the Sewer System Management Plan Update

ISSUE

Shall the City Council adopt Resolution No. 15-32 to approve the Sewer System Management Plan (SSMP) Update as set forth by the State Water Resources Control Board's Waste Discharge Requirements?

RECOMMENDATION

Adopt a Resolution approving City of Holtville's updated Sewer System Management Plan, as mandated by the State Water Resources Control Board.

FISCAL IMPACT

Implementation of the SSMP can be paid from the 2015-16 sewer collection system budget without modification.

DISCUSSION

The City of Holtville's SSMP must be updated and recertified to improve efficiency and enforcement of the City's ongoing program adopted in 2010; and for consistency with the State Water Resources Control Board (SWRCB) Statewide General Waste Discharge Requirements for Sanitary Sewer System (WDRs), Order No. 2006-0003-DWQ, including revised Monitoring and Reporting Requirements, Order No. WQ 2013-0058-EXEC, that went into effect in 2013.

The SWRCB required the City, as a collection system agency, to have adopted and maintain an SSMP, and the governing body must re-approve the agency's SSMP every five years. The purpose of the SSMP is to identify programs and activities that will, when implemented, reduce the number of sanitary sewer overflows (SSOs). The City's original SSMP was certified in November 8, 2010, and the SWRCB deadline for recertification of the updated SSMP is November 8, 2015.

Over the last five years, in accordance with the SWRCB recertification requirement, and to improve enforcement of the City's ongoing program, the Public Works Department has revised the SSMP. The updated SSMP is to be published by the City's website.

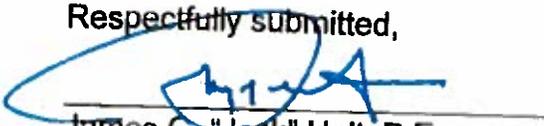
CONCLUSION

The City of Holtville, as a sanitary sewer system collection agency, is required to maintain and periodically updated an SSMP, as well as recertify conditions set forth in the SWRCB 2006 Order.

ALTERNATIVES

No alternatives are to be considered.

Respectfully submitted,



James G. "Jack" Holt, P.E.,
City Engineer

RESOLUTION NO. 15-32

**A RESOLUTION OF THE HOLTVILLE CITY COUNCIL ADOPTING THE
SEWER SYSTEM MANAGEMENT PLAN UPDATE**

WHEREAS, the State Water Resources Control Board (SWRCB) adopted Order No. 2006-003 Statewide General Waste Discharge Requirements for Sanitary Sewer System (WDR) requiring development and implementation of a Sewer System Management Plan (SSMP) that documents a comprehensive program for preventing and addressing sewer system overflows and sewer collection system operation, maintenance, and repair; and

WHEREAS, the City of Holtville developed and implemented an SSMP in November 2010; and

WHEREAS, the SSMP shall be updated every five years and must be approved by the City Council; and

WHEREAS, SWRCB adopted Order No. WQ 2013-0058-EXEC for amending monitoring and reporting program for WDR requiring an update to the SSMP; and

WHEREAS, City Staff has prepared an updated SSMP; and

WHEREAS, the City shall prepare and file a recertification with the State Water Resources Control Board after the adoption of the updated SSMP.

NOW, THEREFORE, BE IT RESOLVED, by the Council of the City of Holtville that it hereby approves the updated SSMP dated October 2015.

PASSED, APPROVED, AND ADOPTED on this 26th day of October, 2015.

ATTEST:

Virginia Ward, Mayor

Denise Garcia, Acting City Clerk

I, Denise Garcia, Acting City Clerk of the City of Holtville, DO HEREBY CERTIFY that the foregoing resolution was duly passed, approved, and adopted by the City Council of said City of Holtville at a regular meeting thereof held on the 26th day of October 2015, and that the same was approved by the Mayor of said City of Holtville on said date, and that the same was adopted by the following roll call vote:

AYES:

NOES:

ABSENT:

ABSTAIN:

Denise Garcia, Acting City Clerk

City of Holtville
REPORT TO COUNCIL

Meeting Date	<u>10/26/15</u>
Item Number	<u>4</u>
Approvals	City Manager <u>[Signature]</u>
	Finance _____
	City Attorney _____

DATE ISSUED: October 26, 2015
FROM: Hector Orozco, GL Analyst
SUBJECT: Resolution No. 15-33 *Investment in FDIC-Insured CDs*

ISSUE

Shall the City Council adopt Resolution No. 15-33 authorizing the Finance Officer, under the direction of the City Manager, to invest funds in federally-insured certificates of deposit to maximize interest earnings on unused cash currently held in deposits at lower rates?

DISCUSSION

The City currently has a Money Market account with First Foundation Bank drawing minimal interest. With approval of the council, the finance departments would like to roll over \$247,250 of those funds into a new CD yielding 1.10%. Although it will tie up fund for a period of 13 months, the rate of return is substantially higher than what is currently being earned. The Federal Deposit Insurance Corporation ("FDIC") insures deposits of up to \$250,000 per institution, so the funds will be protected.

These, along with other funds, need to be continually monitored, reinvested and rolled into new instruments. The resolution continues and reauthorizes that practice.

FISCAL IMPACT

Interest income will be maximized where possible.

CITY MANAGER RECOMMENDATION

It is recommended that the City Council authorize the direct the investment of reserve funds.

ALTERNATIVES

Not to authorize.

Respectfully Submitted,



Hector Orozco

RESOLUTION NO. 15-33

A RESOLUTION OF THE HOLTVILLE CITY COUNCIL AUTHORIZING THE INVESTMENT OF CITY OF HOLTVILLE MONIES INTO CERTIFICATES OF DEPOSIT AND OTHER FEDERALLY-INSURED DEPOSITS

WHEREAS, deposits of cash funds up to \$250,000 in designated institutions are insured by the Federal Deposit Insurance Corporation (“FDIC”) and/or National Credit Union Administration (“NCUA”); and

WHEREAS, reserve funds are typically held by the City of Holtville to fund projected shortfalls and catastrophic events; and

WHEREAS, investment of reserve funds in the Local Agency Investment Fund in the State Treasury has drawn decreasing return on investment in recent years; and

WHEREAS, Federally-insured Certificates of Deposit are available at substantially higher rates of return on such funds; and

WHEREAS, The City Council does hereby find that the deposit and withdraw of money in such Federally-insured instruments for the purpose of investment in the best interest of the City of Holtville.

NOW THEREFORE, BE IT RESOLVED:

- 1) That the City of Holtville hereby authorizes the deposit and withdraw of public monies in such instruments for the purpose of investment therein.
- 2) That the Mayor, Mayor Pro-Tem, City Treasurer, their successors in office and the City Manager shall be authorized to order the deposit or withdraw of monies in these instruments.
- 3) That the foregoing is true, correct and adopted.

PASSED, APPROVED AND ADOPTED by the City Council of the City of Holtville, California, this 26th day of October, 2015

ATTEST:

Virginia Ward, Mayor

I, Denise Garcia, Interim City Clerk of the City of Holtville, California, DO HEREBY CERTIFY, that the foregoing Resolution was duly passed, approved and adopted at a regular meeting of the City Council of the City of Holtville, California, on the 26th day of October, 2015.

Denise Garcia, Interim City Clerk

City of Holtville
REPORT TO COUNCIL

DATE ISSUED: October 26, 2015
FROM: Nick Wells, City Manager
SUBJECT: Resolution No. 15-34 *Waiving the Water Impact Fees for New Residential Water Service at 2076 Mets Road*

Meeting Date	<u>10/26/15</u>
Item Number	<u>5</u>
City Manager	<u></u>
Finance	_____
City Attorney	_____

ISSUE

Shall the City Council adopt Resolution No. 15-34 authorizing the waiver of Water Impact Fees in the amount of \$2,450 for the connection of new domestic water service at 2076 Mets Road?

DISCUSSION

The City was recently contacted by Ronnie Leimgruber regarding the establishment of residential Water Service at a home he recently purchased for his son Andrew on Mets Road. The home is currently served by IID surface canal water.

The residence is located directly along the path of a trunk water line installed by Leimgruber in 2002, off of the City's main service line to the Barbara Worth Country Club and was installed to service the homes he helped develop at the corner of Mets and Claybrook Roads. Although City records are a bit spotty for that period, it has been identified that the project was reviewed and approved by the City Engineer and given the cost of the added infrastructure, impact fees for those homes were waived at that time. It is estimated that Mr. Leimgruber would have paid in excess of \$50,000 for the installation of that line.

With the purchase of the residence in that vicinity, a service line would need to be hooked into the trunk line at the homeowner's expense. Additionally, as the Leimgrubers are aware, the residence will be charged double the base water rate as a connection outside of the city limits. In light of the above, Ronnie and Andrew Leimgruber have requested that the City's waive their Water Impact and Connection Fees to not add further expense to the project.

FISCAL IMPACT

Foregoing the \$2,450 in Water Impact Fees for a Single Family Residence.

RECOMMENDATION

Staff recommends that the resolution be adopted.

ALTERNATIVE

Not to adopt the Resolution and require the payment of fees.

RESOLUTION NO. 15-34

A RESOLUTION OF THE HOLTVILLE CITY COUNCIL AUTHORIZING THE WAIVER OF WATER IMPACT FEES FOR A NEW RESIDENTIAL CONNECTION AT 2076 METS ROAD IN RURAL HOLTVILLE

WHEREAS, the City of Holtville collects Water Impact and Connection Fees for the establishment of new connections to the City's water distribution system; and

WHEREAS, the fees associated with a Single Family/Residential connection equate to \$1,450; and

WHEREAS, such a connection is requested for service at 2076 Mets Road in rural Holtville; and

WHEREAS, the homeowner paid for the installation of Water infrastructure adjacent to the specified residence in 2002; and

WHEREAS, the homeowner intends to install a service connection to that infrastructure at his own expense; and

WHEREAS, the homeowner has requested a waiver of the City of Holtville Water Impact Fees in consideration of the aforementioned expenditures;

NOW THEREFORE, BE IT RESOLVED:

- 1) That the City of Holtville hereby waives the Water Impact and Connection Fees for the connection of service at 2076 Mets Road in rural Holtville.
- 2) That the standard water service rates for service outside of the city limits will apply to this connection.
- 3) The homeowner will be charged the hard cost of the purchase of a residential water meter.
- 4) That the foregoing is true, correct and adopted.

PASSED, APPROVED AND ADOPTED by the City Council of the City of Holtville, California, this 26th day of October, 2015

Virginia Ward, Mayor

ATTEST:

I, Denise Garcia, Acting City Clerk of the City of Holtville, California, DO HEREBY CERTIFY, that the foregoing Resolution was duly passed, approved and adopted at a regular meeting of the City Council of the City of Holtville, California, on the 26th day of October, 2015.

Denise Garcia, Acting City Clerk

Thursday, October 8, 2015 6:06 PM

Andrew Leimgruber
646 Olive Ave.
Holtville, CA 92250
(760)455-9835
andrewleimgruber@Hotmail.com

Nick Wells
Holtville City Manager

To Whom it may concern,

We are currently remodeling a home on Metts Rd. in Holtville's cypher of influence. The property is located at 2076 Metts Rd. This property is adjacent to the new housing lots which have access to Holtville City Water.

We would like to connect our 2076 Metts Rd. property to city water. Also, due to the fact that we funded the infrastructure to expand Holtville's water services, we feel that it would be appropriate to waive the hook up and meter installation and fees. We will be introducing another property that will be paying the higher rate due to its location outside of city limits, and is advantages for Holtville's continued growth and development.

I look forward to discussing this proposal further and I have provided all my contact information for any questions and/or comments.

Andrew Leimgruber



*Trunk Line
added in 2002*

*Proposed
New Service*

*Existing Line
to BWCC*

Claybrook Rd

Claybrook Rd

Meets Rd

Meets Rd

E Bailey Rd

115

580

115

580

115

**MINUTES OF THE REGULAR MEETING OF THE HOLTVILLE CITY COUNCIL AND HOLTVILLE REDEVELOPMENT AGENCY
FEBRUARY 11, 2002**

The regular meeting of the Holtville City Council and Redevelopment Agency was held on Monday evening, February 11, 2002 at 6:00 p.m. at City Hall. Councilmembers present were Doug Byram, Allen Bailey, Victor Gillespie, and Colleen Ludwig. Councilmember Virginia Samaha was absent. Staff present were Gerry Peacher, Charlie Simpson, Peter Mellinger and Jeanne Miller. Charles Cariveau was absent.

CITY COUNCIL MEETING CALLED TO ORDER:

Mayor Bailey called the meeting to order at 6:00 p.m.

PLEDGE OF ALLEGIANCE:

The Pledge of Allegiance was led by Mayor Bailey.

Mayor Bailey
There being
p.m.

Public, N

ments from
the Public Hearing

The Council agreed to table the lot split request in order for the City Engineer to review the plans.

B. CONSIDERATION OF APPROVAL OF REQUEST FROM RONALD LEIMGRUBER FOR WAIVER OF DEVELOPMENT IMPACT FEES IN THE AMOUNT OF \$11,800.00 FOR SIX NEW PARCELS CREATED BY TRACT MAP 00953 A DIVISION OF TRACT 69-70 IN TOWNSHIP 15 SOUTH, RANGE 15 EAST, S.B.M. ACCORDING TO THE OFFICIAL PLAT THEROF – RONNIE LEIMGRUBER

It was moved by Councilmember Ludwig to waive the Development Impact Fees in the amount of \$11,800.00 for the six parcels and there will be no connection fees. The motion was seconded by Councilmember Byram, and unanimously carried.

C. CONSIDERATION OF RESOLUTION NO. 02-03 OF THE CITY COUNCIL OF THE CITY OF HOLTVILLE, CALIFORNIA, AUTHORIZING THE USE OF PROGRAM INCOME REVOLVING LOAN FUNDS TO ASSIST CAMPESINOS UNIDOS, A NON-PROFIT ORGANIZATION IN THE CONSTRUCTION OF 18 AFFORDABLE HOUSING UNITS WITHIN THE CITY OF



CITY OF HOLTVILLE

121 West Fifth Street
Civic Center · Holtville, California 92250 · (760) 356-2912
"The Carrot Capitol of the World"

January 17, 2002

Mr. Terry Barrett, P.E.
B.J. Engineering and Surveying
1850 West Main Street
El Centro, California 92243

**FAX TRANSMISSION: 353-3751
356-1863**

**RE: *City of Holtville Plan Check Review of Waterline Improvements along Mets Road –
Ronnie Leimgruber – 3rd Plan Check (January 17, 2002) THG Project Number 116.137E***

Dear Terry:

I have reviewed the updated plans per the 2nd plan check review comments listed in the Holt Group review letters dated December 13th and 17th, 2001. Please revise the plans as follows:

1. Change the callout note on plan sheet 1 of 3 which states, "Existing 18 inch waterline" to "Existing 8 inch waterline"
2. Please date the revised plans January 17, 2002.
3. Please place a signature block as redlined on the transmitted blue line drawings.

In addition, please provide a copy of the approved Imperial Irrigation District Encroachment Permit for this project. Please forward three (3) sets of original plans to the Holt Group El Centro Office for my signature. One (1) set of original plans will be kept at the Holt Group El Centro Office. The other two (2) sets will be returned to your office. Thank you for your cooperation.

Sincerely,

JAMES G. "JACK" HOLT, P.E.

cc: Ronnie Leimgruber, Developer
John Jordan, City Manager
Gerry Peacher, Public Works Manager
Jeanne Miller, City Clerk
Victor Garcia, The Holt Group
Tim Jones, County of Imperial Public Works Director
Bob Gray, County of Imperial Public Works Department
Jurg Heuberger, County of Imperial Planning Director
Darrell Gardner, County of Imperial, Assistant Planning Director

THE DESIGN ROOM
MARCH 2017 7:00

PARCEL 6



CONTRACT NO. 101123-001
DATE: 03/20/17

PARCEL 4

PARCEL 5

PARCEL 3

PARCEL 2



THE DESIGN ROOM
MARCH 2017 7:00

PARCEL 2

PARCEL 3

PARCEL 5

PARCEL 4



VICINITY MAP

- CONTRACT NO. 101123-001**
- 1. PROJECT NAME: PROJECT NO. 101123-001
 - 2. PROJECT LOCATION: 101123-001
 - 3. PROJECT DESCRIPTION: PROJECT NO. 101123-001
 - 4. PROJECT OWNER: PROJECT NO. 101123-001
 - 5. PROJECT CONTACT: PROJECT NO. 101123-001
 - 6. PROJECT DATE: PROJECT NO. 101123-001
 - 7. PROJECT STATUS: PROJECT NO. 101123-001
 - 8. PROJECT NOTES: PROJECT NO. 101123-001

- LEGEND**
- 1. PROJECT NAME: PROJECT NO. 101123-001
 - 2. PROJECT LOCATION: PROJECT NO. 101123-001
 - 3. PROJECT DESCRIPTION: PROJECT NO. 101123-001
 - 4. PROJECT OWNER: PROJECT NO. 101123-001
 - 5. PROJECT CONTACT: PROJECT NO. 101123-001
 - 6. PROJECT DATE: PROJECT NO. 101123-001
 - 7. PROJECT STATUS: PROJECT NO. 101123-001
 - 8. PROJECT NOTES: PROJECT NO. 101123-001

- REMARKS**
- 1. PROJECT NAME: PROJECT NO. 101123-001
 - 2. PROJECT LOCATION: PROJECT NO. 101123-001
 - 3. PROJECT DESCRIPTION: PROJECT NO. 101123-001
 - 4. PROJECT OWNER: PROJECT NO. 101123-001
 - 5. PROJECT CONTACT: PROJECT NO. 101123-001
 - 6. PROJECT DATE: PROJECT NO. 101123-001
 - 7. PROJECT STATUS: PROJECT NO. 101123-001
 - 8. PROJECT NOTES: PROJECT NO. 101123-001

THE DESIGN ROOM
MARCH 2017 7:00

PROJECT NO. 101123-001

DATE: 03/20/17

submittal area

submittal area

City of Holtville
REPORT TO COUNCIL

DATE ISSUED: October 26, 2015
FROM: Nick Wells, City Manager
SUBJECT: *Complete Streets Consultant Draft Report*

Meeting Date	<u>10/26/15</u>
Item Number	<u>7</u>
Approvals	City Manager <u></u>
	Finance _____
	City Attorney _____

INFORMATION ONLY – NO ACTION REQUIRED AT THIS TIME

DISCUSSION

In 2014, the City was awarded an Environmental Justice Grant through the Department of Transportation to fund a “Complete Streets” study. The purpose of the project is to review and recommend improvement to all forms of mobility in the City. A team of consultants which included representatives of both the Local Government Commission that oversaw the administration of the grant and Alta Planning and Engineering, the consultant tasked to perform the study, visited Holtville several months ago. A variety of workshops and community input sessions led to several interesting preliminary recommendations.

Since then, the consultant has been discussing the possibility of getting Caltrans to allow for amenities to help slow traffic as it enters town from the east on 115. They feel as if they have gotten some traction with Caltrans on a level over the District 11 management, which they were anxious to include in their report to the City. A draft of this report was provided for staff and Council review late last week.

Although staff has not had an opportunity to review and comment on the report, given that the consultant is requesting comments within 30 days, it is being presented to Council in order that their review can be concurrent. Staff comments will be presented at the next City Council meeting in November.

FISCAL IMPACT:

None.

City of Holtville Complete Streets Plan

October 2015



Holtville Complete Streets Plan

Prepared for
City of Holtville

Prepared by
Alta Planning + Design
Local Government Commission
Michael Baker International

October 2015

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Acknowledgements

City Council

Ginger Ward, Mayor
David Bradshaw
Mike Goodsell
Richard Layton
Jim Predmore

City Management

Nick Wells, City Manager
Pete Mellinger, City Treasurer

Special Thanks To

Holtville Unified School District
City Staff
Imperial County Sheriff's Office
Caltrans District 11
Holt Planning Group

Alta Planning + Design

Bryan Jones, PE, AICP Project Manager
Jonathan Schuppert, AICP Assistant Project Manager and Senior Planner
Laurentiu, PE, Engineer
Evan Burbage, Designer
James Powell, Designer
Leah Stender, AICP, Planner
Lola Torney, Planner

Local Government Commission (LGC)

Paul Zykofsky, Associate Director
Anthony Leonard, Project Manager

Michael Baker International

Howard Blackson, Urban Design and Community Planning Manager

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1. Executive Summary

A Commitment to People

The objective of the Holtville Complete Streets Plan is to move and connect people, schools, and businesses by enhancing the built environment for people who ride a bike, walk and use transit to create safer, healthier, and a more economically prosperous and connected community.

This Plan provides to tools needed to rework those resources, ingredients, and information that have been around for a long time and turning them into new models that make things more valuable.

Overview

Holtville, the “Carrot Capital of the World,” is a city in Imperial County and also serve as a stop between southeast and southwest California (although not along a predominant interstate route). In 2013, over 6,000 residents called the 1.1 square mile city home.

The Holtville Complete Streets Plan provides for a recommended citywide active transportation network of bicycle paths, pedestrian improvements, and support facilities intended to ensure active transportation continues to be a viable transportation option for people of all ages and abilities to live, work, and play in Holtville.

Bicycling and walking offer residents a number of benefits, including improved health, reduced air pollution, and reduced traffic congestion. These benefits, combined with Holtville's generally flat terrain and number of sunny days, make bicycling and walking a viable form of transportation and an enjoyable recreational activity. However, in many cases, roadways are too wide for the number of vehicles served, costing the City extra money for maintenance. Reconfiguring some roadways and repurposing other roadway elements will make Holtville feel more like a city for people rather than cars. In addition, repurposing excess pavement can reduce maintenance costs. This can be done opportunistically as repaving occurs and other capital improvement projects are constructed.

Plan Process

The City of Holtville Complete Streets Planning process kicked off with meeting on February 24, 2015 to discuss the project and public participation. A public meeting was held on March 30, 2015. After an initial round of targeted public input, extensive field work, walk audits, and data collection was conducted by



Figure 1-1: City staff, police, and consultants conducted walk audits to find target areas of improvements

consultants and City staff, as shown in Figure 1-1. Potential solutions and recommendations were found and presented in a final public meeting on April 2, 2015. These recommendations are shown in detail in Chapter 4: Recommendations.

Plan Function

The City of Holtville recognizes the value of active modes of transportation to improve the pedestrian and bicyclist environment and to establish itself as a more walkable, bikeable, livable, and healthy city. This Plan outlines a broad vision, strategies, and actions for improving a quality of life in Holtville for people of all ages.

The Plan serves as a way for Holtville to seek funding for complete streets projects outlined in Chapter 4: Recommendations and Chapter 5: Implementation Plan. Having an adopted plan is helpful when applying for funding from state, federal, and private agencies. Adopting this Plan does not commit the City to dedicate or allocate funds, but rather indicates the intent of the City to implement this plan over time, starting with the priority recommended projects.

Action Steps

1. Seek Multiple Funding Sources

Multiple approaches should be taken to facilitate complete street development. It is important to secure the funding necessary to undertake priority projects and to develop a long-term funding strategy to allow continued development of the overall system. Capital and local funds for sidewalk and crosswalk construction should be set aside each year – even if only for a small amount – as local funding can be matched to outside funding sources. A variety of local, state, and federal options and sources exist and should be pursued. These funding options are described in Chapter 5: Implementation Plan.

2. Improve City Policies Regarding Pedestrians and Bicyclists

The City already has a set of policies regarding "alternative transportation" and infrastructure, primarily found in the City's Circulation Element of the General Plan (2003). This Plan recommends policy updates and additions to further enhance the City's commitment to all modes of transportation.

3. Implement Priority Recommended Projects

Holtville has many gaps in its pedestrian and bicycle network and areas where pedestrian and bicycle usage is limited because of automobile circulation. Providing better pedestrian and bicycle facilities has shown to quickly increase walking and bicycling within a community. Addressing near-term key gaps, with targeted

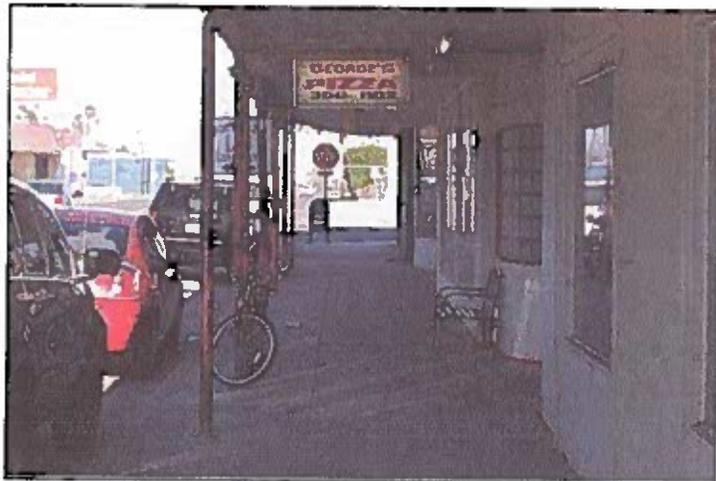


Figure 1-2: Walkway with bicycle parked along Fifth Street

investment, facilities, and traffic calming, will generate substantial benefit to residents across Holtville.

4. Review Plan Progress

During annual budget preparation or when funding categories become available through grants, the City should review progress and determine next implementation priorities. The City may also decide to reevaluate progress and make plan updates as needed.

Priority Projects

This Plan presents a series of projects that provide new approaches and guidelines for future projects. The top three recommended projects are:

- East Gateway Monument and Crossing
- Curb Extensions at Holt Park
- Curb Extensions at Fifth Street and Cedar Avenue

The full range of recommended projects are presented in **Chapter 4: Recommendations**.

Plan Outline

The full Plan chapters are as follows:

1. Executive Summary (this chapter)
2. Background
3. Community Engagement
4. Recommendations
5. Implementation Plan
6. Recommendations Toolkit



Figure 1-3: A boy skateboarding along Sixth Street

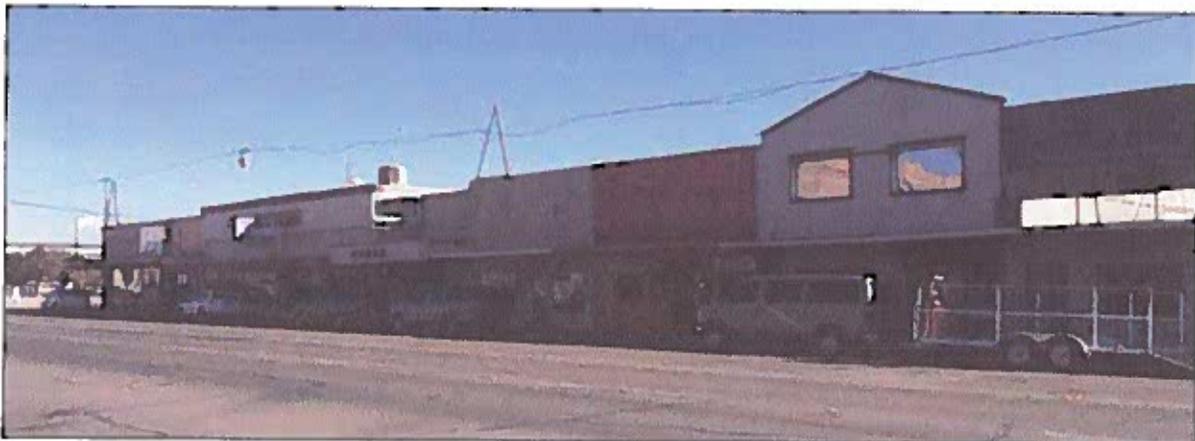


Figure 1-4: Fifth Street Holtville, CA

Complete Streets Goals

Complete streets simply put are streets that accommodate all roadway users. Complete streets come with many benefits, such as:

- Creating welcoming and inviting streets
- Improving quality of life
- Balancing moving people, not just cars
- Encouraging walking, biking, and public transportation
- Enhancing safety
- Enhancing economic vitality

These benefits are essentially the goals identified throughout the community engagement and plan preparation processes.

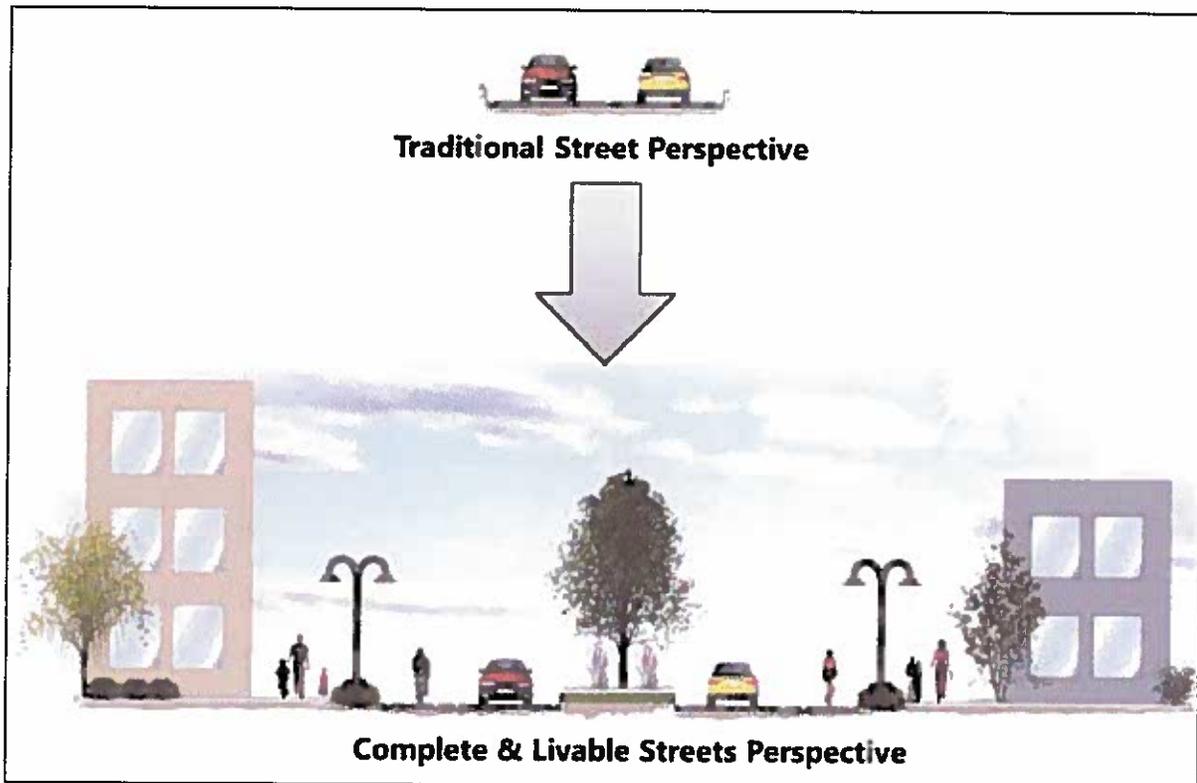


Figure 1-5: Complete Streets Perspective

2. Background

Incorporated in 1908, the City of Holtville is a rural community in Imperial County with an estimated population of 5,969 (2009-2013 American Community Survey) and an additional 1,274 in the rural outskirts (City of Holtville Sphere of Influence Statistics). California State Route 115 (also known as SR 115 or Fifth Street) was the original east-west interstate artery connecting California and Arizona. The construction of Interstate 8 (approximately 2.5 miles south of and parallel to SR 115) in the 1960s bypassed Holtville and became the major east-west interstate artery, carrying approximately 11,500 motorized vehicles as of 2012. While a state highway facility, SR 115 now serves more regional and local traffic as is demonstrated in its relatively low traffic volume of less than 7,000 motorized vehicles as of 2012.

Streets and Blocks

The city is built on a block-level grid system with the majority of buildings, parks, schools, and other uses on or north of Fifth Street, although new developments have occurred south of Fifth Street adding parks, housing, and jobs. Streets generally consist of wide travel/parking lanes and relatively narrow sidewalks with and without parkway strips. Most streets could be considered “incomplete” in that they are designed primarily for travel by motor vehicles with little consideration for pedestrians, cyclists or transit. The harsh desert climate and deferred maintenance have taken its toll on the pavement causing buckling and cracking. The excessive width can encourage speeding, increase the urban heat island effect, and increase maintenance costs for the City.

Excessively wide streets, while they may accommodate additional capacity for through-moving or parked vehicles, significantly increase costs for cities, create barriers for pedestrians to cross, increase vehicular speeds which can lead to more enforcement and more severe injuries (even death) if a collision occurs, creates poor utilization of infrastructure, and creates higher development impact fees, which reduces the likelihood of new development.

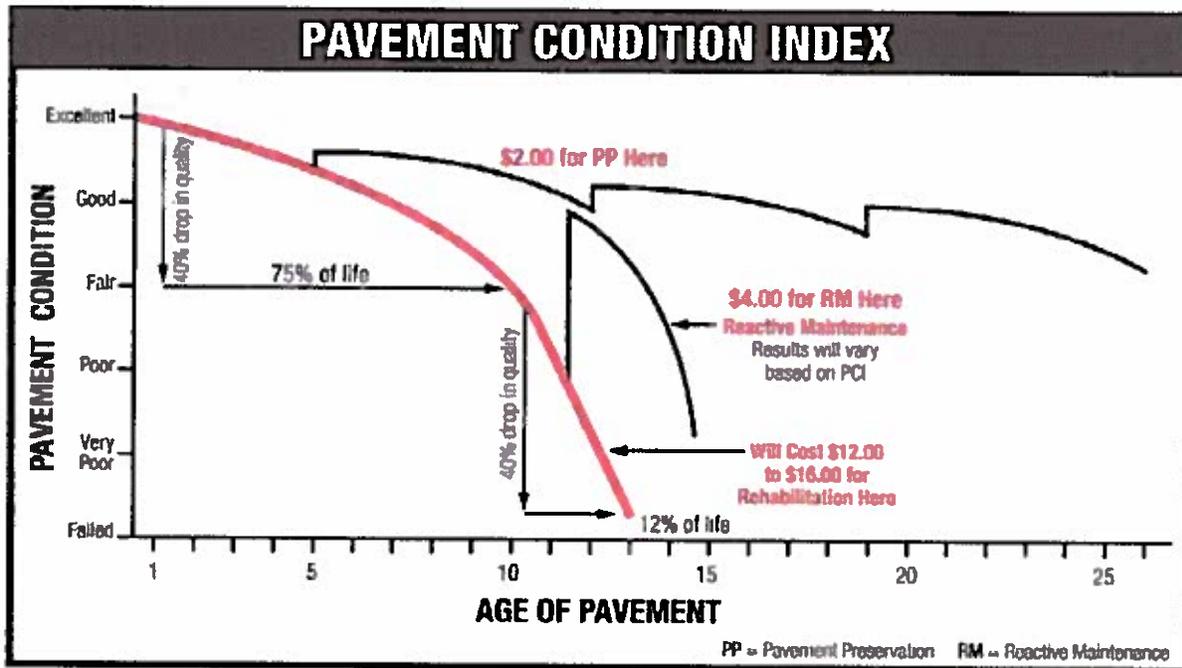


Figure 2-1: Pavement Condition Index

Schools

A recent survey of elementary school parents found that 55 percent lived within a quarter mile from school, but less than 30 percent of the students walked or bicycled to school. Parents generally cited distance, safety at intersections and crossings, and amount/speed of traffic along the routes as the reasons for students not walking or bicycling to school. As with other parts of California and the nation, childhood obesity rates are high in Imperial County with 41 percent of children 5-20 years old found to be overweight or obese in 2010 (Pediatric Nutrition Surveillance System).

Community Engagement

The Local Government Commission (LGC), Alta Planning + Design, and Michael Baker International, in partnership with the City of Holtville (City) and the California Department of Transportation (Caltrans), conducted field work, facilitated a series of community engagement activities, and prepared this report to address the critical needs of the community to identify existing conditions and develop implementation strategies and priorities to create more complete streets. Refer to Chapter 3: Community Engagement for additional information.

Vehicle Speeds and Lane Configuration

SR 115 is a rural highway on either side of the city limits and transitions into a city street. From the west, SR 115 is a four-lane divided highway with posted speed limits at 55 MPH. Speeds quickly transition to 35 MPH and 25 MPH as the highway transitions into a city street with buildings and sidewalks adjacent to the roadway. Speeds increase to 35 MPH between Walnut Avenue and Maple Avenue and to 55 MPH around Grape Avenue where the roadway transitions into a two-lane undivided highway. East of Grape Avenue, the street character transitions to

rural/agricultural on the north side and on the south side the transition occurs after a large apartment complex.

Observed speeds from the west are generally lower at the approach to the city than from the east, due to the sharp curve onto Fifth Street at Palm Avenue. There are no physical changes to the roadway from the east approach as there are on the west approach. In fact the roadway widens from the east providing a greater design speed and capacity near Grape Avenue where a trail crossing and suggested route to school crossing is located. Speeds entering town from the east were generally observed to be higher than that of the west approach. Likewise, speeds leaving town were generally observed to be higher.

Figure 2-2 illustrates the existing lane and speed configuration of SR 115.

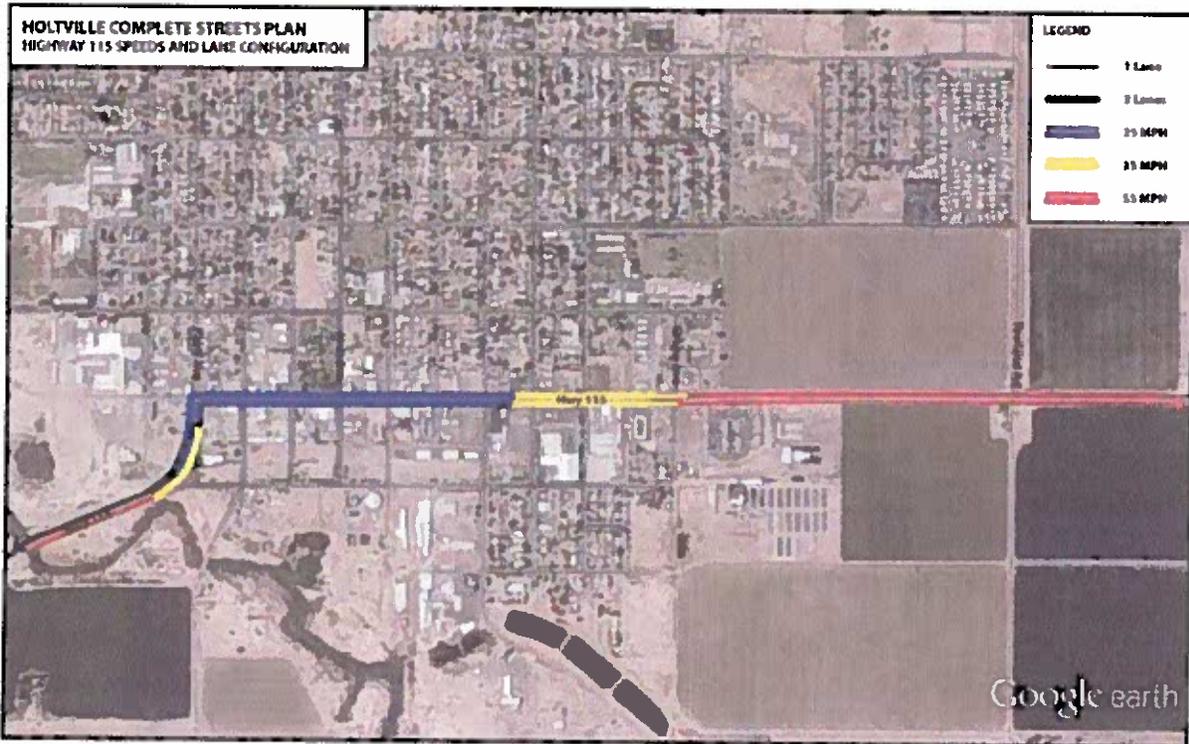


Figure 2-2: SR 115 Speeds and Lane Configuration

3. Community Engagement

Overview

This project was centered on a design charrette process held from March 30 to April 2, 2015 in the City of Holtville. Charrettes are community-based design exercises that come out of a sincere intent to have the public involved in a meaningful way to craft their own future. This format allows residents, users of a street, or whatever population is targeted to be the primary force behind the designs.

Several partner organizations were involved in this project. City of Holtville Staff provided oversight and guidance for the project. Local Government Commission (LGC) staff handled tasks related to managing the project team, development of the schedule, and in leading the outreach to community members. Alta Planning + Design (Alta) and Michael Baker International provided the main transportation engineering and design services for developing the plan and graphics used throughout the charrette.

From November 2014 to February of 2015, two advisory group meetings were held to help guide the project partners with outreach and plan development. During that time, project team members from the LGC and Alta Planning + Design conducted an initial review of the issues of concern for the City of Holtville, and led site visits to better understand the existing street conditions.



Figure 3-1: The pre-charrette visit revealed a variety of bicyclists using SR 115



Figure 3-3: The pre-charrette visit revealed issues with current infrastructure

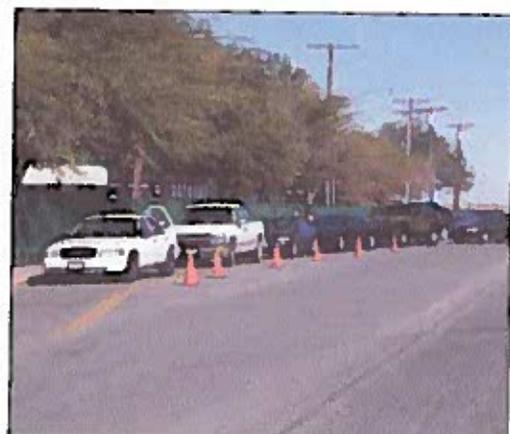


Figure 3-2: The pre-charrette visit revealed well-organized SRTS practices, such as this student pickup at Finley Elementary School

Outreach Methods

Several outlets were used to conduct outreach to the community for this project. English and Spanish-language materials were distributed to help publicize the events for the design fair activities. Outreach efforts included:

- English and Spanish language flyers were posted at several locations throughout the City such as City Hall and other City offices, School District the Meyer Memorial Library, churches, Holtville Chamber of Commerce, local shops, health centers, and other public venues. The flyers were also sent home with students at the Finley Elementary School. All charrette events, as well as the Safe Routes to School workshop, were advertised in the flyer.
- Media releases were sent to area papers, and an advertisement for the charrette events was run for two weeks in the Holtville Tribune leading up to the charrette.
- Emails with digital copies of the flyer were sent to elected officials, staff, local organizations, and others to encourage participation and spread the word.
- City and project staff conducted direct talks with some local business owners and others about the upcoming workshops prior to the charrette.

Attempts were also made to work through the school district and other organizations to reach out to Spanish-speaking residents and other community members.

Design Charrette Activities

From March 30 to April 2, 2015 the design team held various public events in English and Spanish to engage the community and solicit input for the plan.

Talleres sobre Plan para Calles Completas de Holtville

¿Cómo podemos asegurar que nuestros calles funcionen para todos los usuarios que necesitan, utilizan o desearían utilizar?

Objetivos de este taller

- Para mejorar la seguridad de la Calle 4
- Para fomentar desarrollo económico
- Para asegurar la seguridad para peatrones, ciclistas y conductores
- Para mantener el carácter histórico de la comunidad

Hágalo por un caso o más de los siguientes eventos

Calles Completas
Lunes, 30 de marzo, 2015
Asamblea posterior
• 5 - 6 pm - Parque de Holt

Taller de Diseño de Apertura
• 6 - 8 pm - Centro Cívico de Holtville
Habrán refrigerios ligeros

Jueves, 2 de abril, 2015
Presentación de Recomendaciones
• 6-7:30 PM - Centro Cívico de Holtville
Habrán refrigerios ligeros

Rutas Seguras a la Escuela
Miércoles, 31 de marzo, 2015
Taller para crear Rutas Seguras a la Escuela
• 8:15 - 11:15 am
Escuela primaria Finley, Cuarta 22
Habrán refrigerios ligeros

TODOS LOS EVENTOS SE CELEBRARÁN EN INGLÉS Y ESPAÑOL

Para más información por favor llame al (314) 441-2300 ext 111 o visite dotpln.org

How should we balance our streets for all people who walk, drive, or bike, in Holtville?

HOLTVILLE COMPLETE STREETS DESIGN WORKSHOPS

Monday, March 30
Guided Walking Tour
5:00 - 6:00 pm

Community Design Workshop
6:00 - 8:00 pm

Thursday, April 2
Closing Presentation of Plan
6 - 7:30 pm

Events at:
Holtville Civic Center, 121 W. 5th Street

Join the conversation! Events will be held in Spanish and English. Refreshments will be provided.

For more information about either project activities, contact Tony Leonard at 936-446-2298 x315 or afleond@dotpln.org

Figure 3-4: The charrette flyer was offered in Spanish and an ad ran in the local newspaper

Technical Focus Group

Early in the week, the project team held a technical focus group with City Staff, members of the Holtville City Council and Planning Commission, Imperial County Transportation Commission, and the Southern California Association of Governments. Caltrans District 11 staff were invited, but unable to attend the March 30th meeting. However, the project team met with Caltrans after the charrette to provide an update on the charrette, discuss the plan outline, and review potential recommendations.



Figure 3-5: Meeting with City staff and regional agencies

Walking Tour

Prior to the first workshop on March 30, 2015, team members led a walking tour with participants along Fifth Street and side streets in downtown Holtville. They observed and discussed existing land uses and street conditions, including design, walkability, traffic patterns, intersections, crossings, sidewalk conditions, and other features. During the walk, a near collision was witnessed at the corner of Fifth Street and Holt Avenue, the one intersection with a stop sign. Participants discussed issues with the current configuration and possible treatments to remedy these. After the walk they regrouped at the Civic Center for refreshments and to discuss Complete Streets principles and possible design solutions for the City.

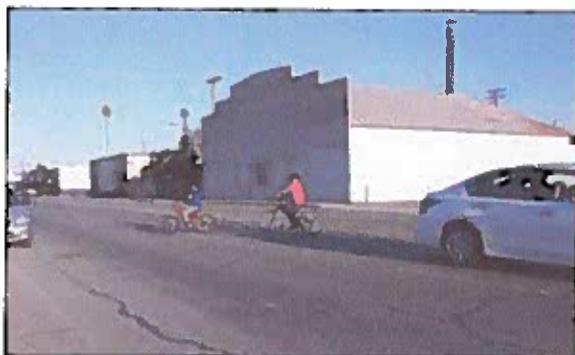


Figure 3-6: These kids show the walking group what bicyclists can face on SR 115



Figure 3-7: Discussing engineering options for the intersection

Opening Presentation and Community Design Workshop

The opening community workshop for the design charrette process was held on the evening of March 30 at the Holtville Civic Center. City Manager Nicholas Wells welcomed participants to the workshop. Paul Zykofsky, Associate Director at the LGC, and Bryan Jones, Senior Associate Engineer at Alta provided background on the Complete Streets Plan project and illustrated the principles of Complete Streets with examples from other communities.



Figure 3-8: Participants during the opening workshop

After the presentation the design team asked participants to break up into smaller groups for a design exercise. For this exercise everyone was asked to identify critical issues on large aerial maps of the city, as well as put down some of their own street design solutions. Due to the small size of the group attending, participants ended up working in one group. However, they held energetic conversations as they discussed problems, and alternative

solutions. During this exercise, project team members joined the group to observe, commenting if appropriate, and answering questions when asked.

Safe Routes to School Workshop

Since one of the goals was to work with the schools in Holtville to look at Safe Routes to School (SRTS) issues, LGC staff offered a workshop in English and Spanish on SRTS for parents and staff at Finley Elementary School on March 31. Attendance for the event did not reach the desired level of participation. However, the LGC still conducted the presentation on SRTS, and tried to capture input from those that showed up.

Prior to the workshop, staff from Alta Planning + Design distributed information about upcoming charrette events to parents and students. Staff also witnessed school drop off procedures and spoke with crossing guards, teachers, and others as available.



Figure 3-9: Presenting on Safe Routes to School at Finley Elementary School

Spanish-Language Popup Activity



Figure 3-10: Taking the charrette directly to Spanish-speaking residents

Spanish-language engagement was incorporated into the charrette process. Since attendance of the opening workshop by Spanish-language speakers was low, LGC staff improvised and decided to go directly to Spanish-speakers on the street. They grabbed a table and aerial map of Holtville, and put together a popup activity outside the Del Sol Market. This offered passers-by the opportunity to voice their concerns and identify safety improvements they would like to see made to make Holtville's streets work better for everyone. This approach also allowed for engaging dialog.

Design Team Working Sessions

The design team started the week taking measurements and photos of the study area, and observing how the street network functioned. After gathering initial input from the opening workshop and technical focus group meeting, the design team started refining draft recommendations for this Plan. During this time, the design team furthered concepts and maps to reflect existing conditions and test recommendations for improvements.

The design team spent three days collating the concepts developed from the public input opportunities and preparing draft recommendations for the closing session presentation. This included ongoing discussions between team members and City staff along with additional site visits as needed for fact checking and

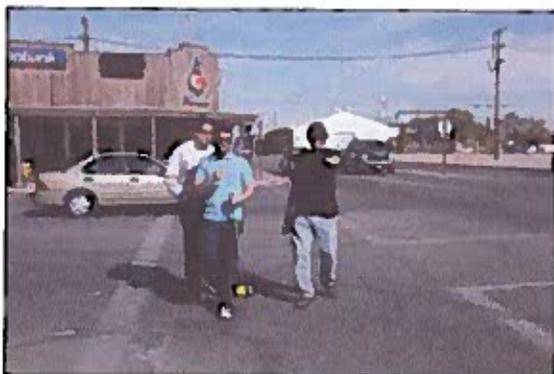


Figure 3-11: Team members documented conditions on the ground

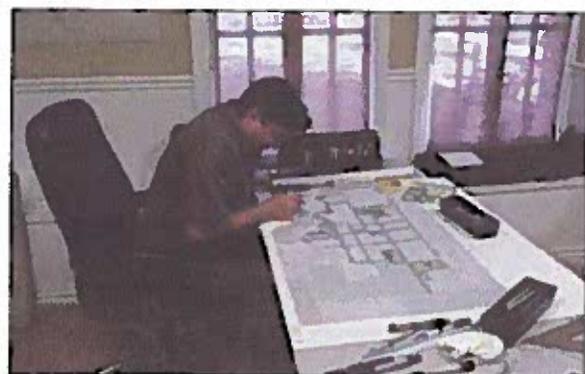


Figure 3-12: Team members worked on drawing up maps for improvements



Figure 3-13: Team members refining engineering solutions

Presentation of Initial Recommendations

On April 2, 2015, the design team held a public workshop at the Holtville Civic Center to present the first preliminary draft recommendations to residents. Paul Zykofsky of the LGC and Bryan Jones of Alta Planning + Design reviewed key findings from previous public events and shared illustrations of initial recommendations, including before and after visuals of potential changes. A question and answer period and open discussion started at the conclusion of the presentation.

After this workshop, the design team began drafting the Plan. Input gathered from the charrette has helped form the basis for recommendations in this Plan.

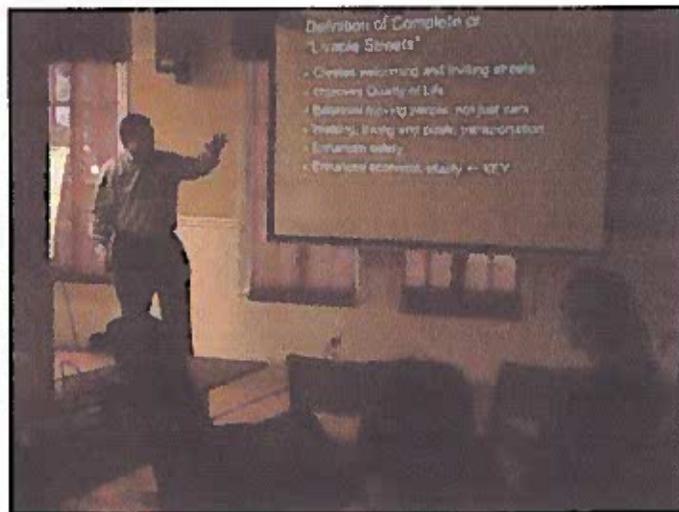


Figure 3-14: Presenting initial recommendations

Subsequent meetings were held on May 4, 2015 and June 15, 2015 with Caltrans District 11 staff to outline the report, receive final project recommendations, and address issues related to proposed recommendations. These meetings mostly focused on issues around and proposed solutions for the east gateway.

What We Learned:

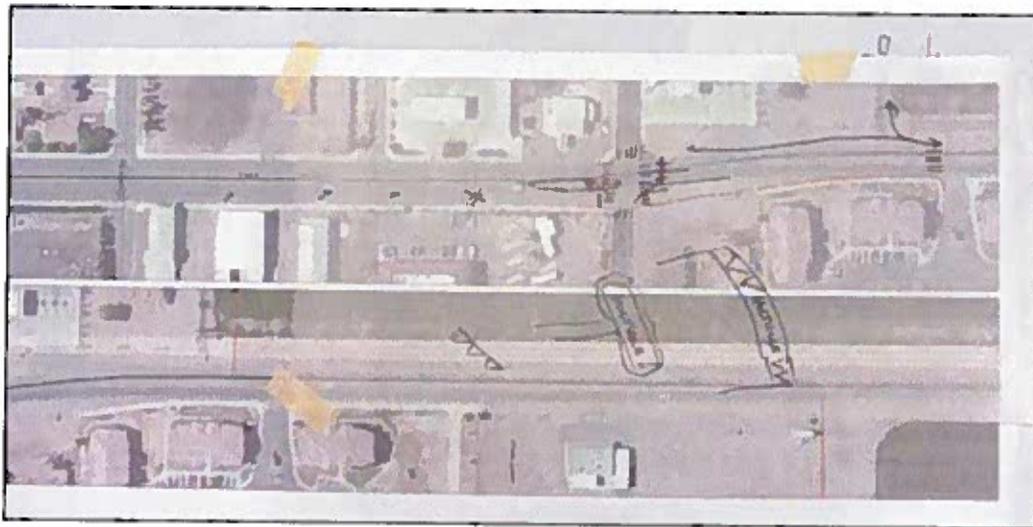
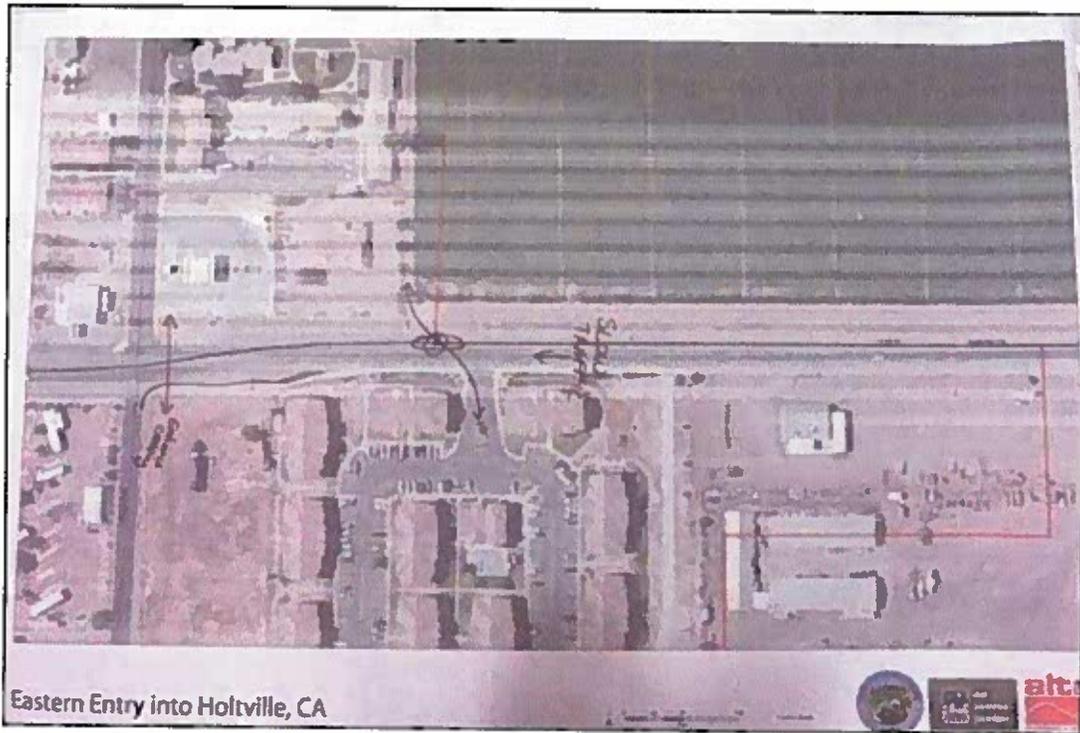
- The City has a well-established grid.
- Homes generally front north-south avenues.
- Alleys access on east-west streets.
- Holt Park is the center of community and a focal point for economic opportunities.
- Businesses front Fifth Street & Holt Park
- Some buildings are near the sidewalk and others are set back with parking lots in front.
- Strengthen connections to places and trail.
- Seventh Street connects all three schools and two parks.
- Shade from trees & buildings make a more comfortable environment for people walking.
- Marked crossings and enhanced safety designs are vital across Fifth Street.
- Crossing Fifth Street is challenging because of width and speed.
- Change takes time and costs money.
- There are temporary and cost effective solutions.

Summary of Community Input from Aerial Map Exercises

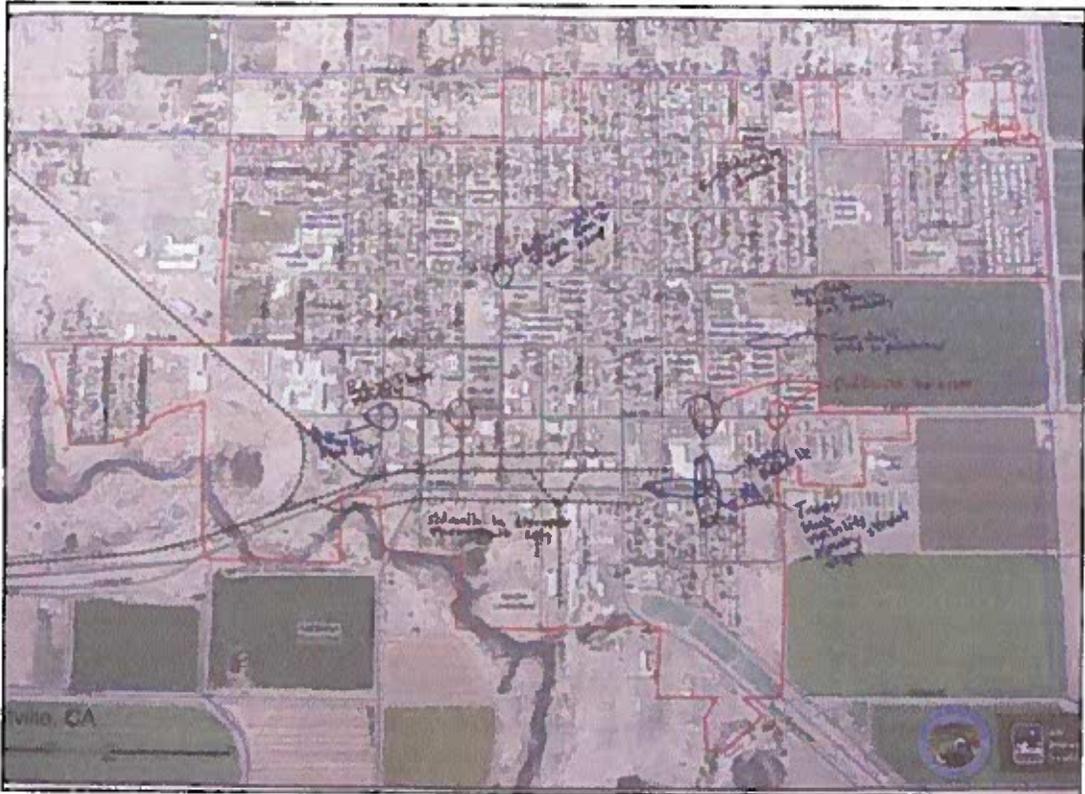
- Slow traffic on eastern end of City.
- Grape Avenue and Chestnut Avenue are difficult to cross.
- There is poor visibility at the corner of Cedar Avenue and Fifth Street.
- There could be angled parking on Cedar Avenue near Fifth Street.
- Add curb extensions around Holt Park and better crossings.
- Paint red curb in front of Hot Rods and Beer.
- Possible 2-Way left turn lane on Fifth Street.
- Use striped crossings on Fifth Street.
- Construct a better gateway sign on east end of Fifth Street.
- Add missing sidewalks and fix where in disrepair.
- Sometimes parking blocks visibility at corners.
- Sometimes trees block visibility at corners.
- Some street signs are missing.
- Olive Avenue and Fifth Street are difficult to cross. Need a stop sign at intersection.
- Need better lighting.
- Cars don't always yield to pedestrians near schools.
- Steep valley gutters at some intersections function as traffic calming but also create challenges for pedestrians and bicyclists to navigate.

Maps after Charrette





Holtville Complete Streets Plan



4. Recommendations

Based on requests from City staff, public feedback, and coordination with Caltrans, this chapter identifies recommendations to transform Holtville roadways into “complete streets” by providing safe connections between destinations for all roadway users. Many recommendations can be done in phases. Phasing allows costs to be absorbed over time and allows City officials to test recommendations with lower cost options before making permanent or more costly improvements.

Complete streets simply put are streets that accommodate all roadway users. Complete streets come with many benefits, such as:

- Creating welcoming and inviting streets
- Improving quality of life
- Balancing moving people, not just cars
- Encouraging walking, biking, and public transportation
- Enhancing safety
- Enhancing economic vitality

These benefits are essentially the goals identified throughout the community engagement and plan preparation processes.

Relinquishment

Many community members identified Fifth Street as a barrier to safety due to the speed of motorists, the challenges for pedestrians to cross, and the lack of a more walkable community core. This Plan recognizes that this roadway has excess capacity and may not need four travel lanes (two in each direction) given the development of I-8 that serves as the regional bypass to the City of Holtville.

There is only one all-way stop sign through town on Fifth Street at the intersection with Holt Avenue. Six marked crosswalks are provided in the 11 intersections between Palm Avenue and Grape Avenue. Many curb ramps and crosswalks do not meet ADA or Caltrans standards as one crosswalk currently leads pedestrians to another crosswalk rather than the sidewalk (Figure 4-1), are poorly maintained (Figure 4-2), and as a result are not as visible as they could be to alert motorists transition from a rural highway to a community with people.



Figure 4-1: Enhanced aerial view of Fifth Street at Holt Park shows a crosswalk leading to another crosswalk, which do not conform to Caltrans standards



Figure 4-2: Poorly maintained roadway infrastructure along Fifth Street

Caltrans has expressed interest in relinquishing SR 115 to be a locally-operated roadway as it is not needed as a state facility now that Interstate 8 serves that purpose: according to the SR 115 Transportation Concept Report by Caltrans District 11 (2011), the existing SR 115 alignment between I-8 and West Junction Evan Hewes Highway is to be relinquished by 2025. And according to Chapter 25 of the Caltrans Project Development Procedures Manual (2014), there are several benefits to relinquishing facilities that are no longer required to serve regional and statewide needs, including an increase in local agencies' responsiveness to community interest in the administration, planning,

construction, and operation of facilities, resulting in a cost savings to taxpayers by eliminating the need for State encroachment permits. The City has expressed challenges with adequately maintaining its existing infrastructure and may not be able to take on additional infrastructure. However, if SR 115/Fifth Street were to become a locally-operated roadway, there would be significantly greater flexibility and local decision making ability to utilize state of the practice solutions, implement safety measures, and spur economic development along the corridor.

There are multiple options available to implement complete streets practices on Fifth Street. One option is to draft a memorandum of understanding (MOU) between Caltrans and the City of Holtville that would allow for relinquishment of the roadway without meeting full Caltrans standards. Another option is to implement recommendations from this Plan without updating any part of Fifth Street, and rely on Caltrans to maintain the roadway and add bicycle and pedestrian facilities over time. Holtville and Caltrans could also enter into a partnership or opt to split the costs of the recommended upgrades along Fifth Street.

While Caltrans offers relinquishment to local jurisdictions through a California Transportation Commission resolution, the roadway must adhere to Caltrans standards to qualify for relinquishment and be brought up to acceptable conditions of repair. The initial step would be for Caltrans District 11 System Planning staff to prepare a Relinquishment Assessment Report (RAR) to determine if a relinquishment is appropriate and feasible from a State Highway System perspective. During the preparation of a RAR for SR 115, an MOU should be drafted between the City and Caltrans to allow for improvements to Fifth Street prior to relinquishment. Due to lack of available funds, the MOU would allow for relinquishment without meeting Caltrans standards. Instead, the roadway need only meet state of the practice standards.

Another option is for Caltrans to continue to maintain the roadway and upgrade bicycle and pedestrian facilities per Caltrans Main Street Guidelines over time. Any updates around Holtville that affect Fifth Street would require a Caltrans encroachment permit due to traffic control.

The last option is for the City and Caltrans to split costs of upgrades along Fifth Street, with the understanding that streets serve everyone in a community regardless of which agency owns and maintains the roadway. This follows Caltrans policy G3-P3 and strategy P3-S10 from the California Transportation Plan 2040 (2015). The policy states to "seek sustainable and flexible funding to maintain and improve the system." Strategy 10, in order to support and

achieve that policy, states that Caltrans will “promote flexible funding for transportation problems that have significant public benefits, regardless of facility ownership and/or jurisdiction.” Repurposing Fifth Street would benefit thousands of people and would help Caltrans act upon its adopted policies and mission to provide a safe, sustainable, integrated, and efficient transportation system to enhance California’s economy and livability.

For all recommendations in this chapter, if Caltrans and the City cannot come to a mutual agreement, changes could only be installed on the non-Caltrans roadways.

Curb Extensions at Cedar Avenue

The community expressed challenges of crossing Fifth Street at Cedar Avenue. Many cross the street here to go between businesses and to get to parked vehicles. This Plan recommends to add curb extensions to the corners of Fifth Street at Cedar Avenue. This can be done in two phases based on funding availability.

Existing Conditions

Cedar Avenue is an unprotected intersection with a fair amount of truck traffic. The crossing distance across Cedar Avenue on Fifth Street is 50 feet. The crossing distance on Cedar Avenue across Fifth Street is 70 feet. Cedar Avenue has parallel parking along the whole block. The community expressed concerns regarding vehicles parked on SR 115/Fifth Street directly in front of the business and block the view for drivers wishing to turn left onto Fifth Street from Cedar Avenue. **Figure 4-3**, **Figure 4-4**, and **Figure 4-5** show the current intersection configuration. Most of the curb ramps are not ADA compliant or up to current standards.



Figure 4-3: Current intersection design at Cedar Avenue and Fifth Street



Figure 4-4: Northeast corner of Cedar Avenue and Fifth Street, looking east

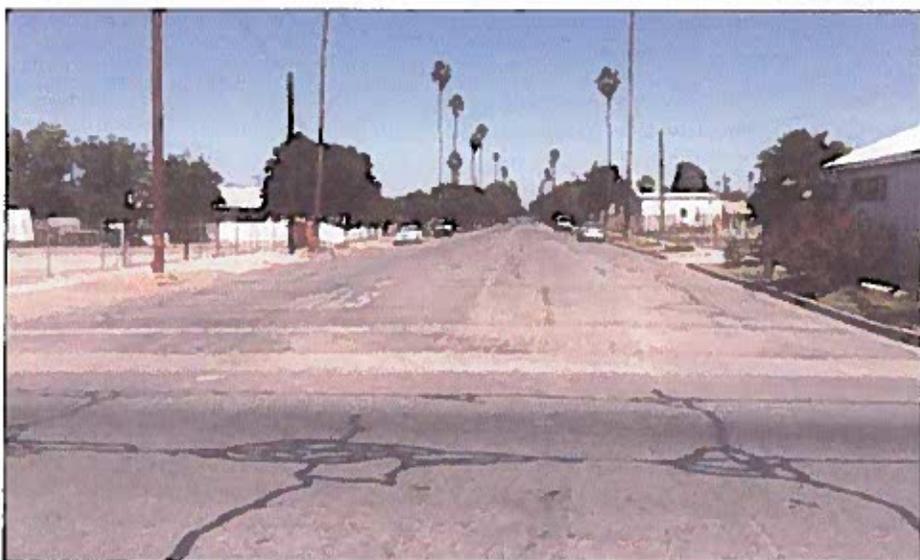


Figure 4-5: Looking north at Fifth Street and Cedar Avenue

Recommendation

This Plan recommends reconfiguring the northern corners of Cedar Avenue and Fifth Street. This can be done in two phases based on funding availability. The first phase should add continental or ladder-style crosswalks to this intersection. This will alert drivers to the possible presence of pedestrians; see [Figure 4-6](#), [Figure 4-7](#), and [Figure 4-8](#). This crosswalk style is a Caltrans standard in the California Manual on Uniform Traffic Control Devices (CA MUTCD).

It is also recommended to convert parallel parking along Cedar Avenue (Sixth to Fifth Streets) to angled parking, to accommodate more parking, better utilize the excess roadway, and promote business access.

A partial red-painted curb for fire access should be added in front of the business on the northeast corner of Cedar Avenue and Fifth Street. The red curb will enhance safety of sight distance.

A bicycle corral can be added near the intersection to increase bicycle parking availability in Holtville. Caltrans allows intersection corner bicycle parking as it does not interfere with vehicle sight lines.

Non-ADA-compliant and substandard curb ramps should be improved to meet the requirements of the ADA and current standards, including truncated domes, appropriate slope, and landing area.



Figure 4-6: Intersection of Fifth Street and Cedar Avenue with continental crosswalks



Figure 4-7: Northeast corner of Fifth Street and Cedar Avenue with painted crosswalks and bulbouts



Figure 4-8: Intersection of Cedar Avenue and Fifth Street after phase one is complete, looking north

The second phase will add permanent - and possibly landscaped - bulbouts at these corners. **Figure 4-9** and **Figure 4-10** shows possible bulbout designs.

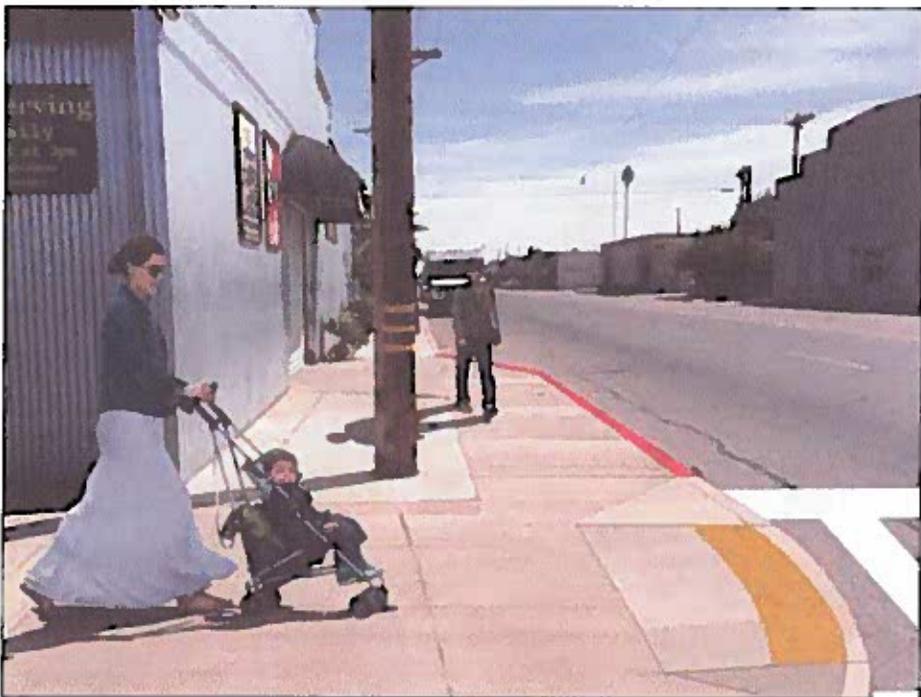


Figure 4-9: Permanent bulbouts will reduce crossing distances and increase sidewalk space for pedestrians



Figure 4-10: Looking north at Cedar Avenue and Fifth Street after second phase is complete

Curb Extensions at Holt Park

Holt Park was repeatedly stated as the heart of the city by many community members. This major gathering hub for residents is also home to many City services. Access to the park was identified as a challenge due to crossing distances and motor vehicle speed at corners. This Plan recommends to add curb extensions on all corners of each intersection surrounding Holt Park to reduce pedestrian crossing distances, improve pedestrian visibility, and reduce motor vehicle speeds. Affected intersections are:

- Sixth Street at Pine Avenue
- Sixth Street at Holt Avenue
- Fifth Street at Holt Avenue
- Fifth Street at Pine Avenue

As with the Cedar Avenue reconfiguration, these recommendations can be done in phases based on accessible funds.

During a charrette walking tour, participants witnessed a near collision at the all-way stop at Fifth Street and Holt Avenue, caused in part by a lack of dedicated left-turn lanes. An eastbound left-turning vehicle and a westbound through-moving vehicle nearly collided as it was not evident that one of the vehicles would be turning left. To resolve this issue providing dedicated left-turn lanes so on-coming motorists know that a vehicle in that lane is turning is advised.

Existing Conditions

Crosswalks on Holt Avenue at Fifth Street are approximately 89 feet in length. The poor pavement condition caused over time from weather and deferred maintenance makes it difficult to cross. Any upgrades would involve repaving the crosswalk area, as shown in Figure 4-11. Additionally, the current crosswalk design is unconventional and not compliant with Caltrans standards, as shown in Figure 4-12. Many curb ramps do not meet ADA or Caltrans standards, and should be reconstructed with improvements. In addition, best practices would suggest installing crosswalks across all approaches of an intersection to reduce exposure and distance a pedestrian has to cross.



Figure 4-11: Poor crosswalk condition around Holt Park



Figure 4-12: Aerial view of Fifth Street at Holt Park

On the north side of Holt Park, pedestrians must walk over 100 feet within the crosswalk to cross Pine Avenue on Sixth Street. Crossing Sixth Street along Holt Avenue is 90 feet. Figure 4-13 shows crosswalk configuration north of Holt Park. Figure 4-14 shows the intersection design on the ground.



Figure 4-13: Aerial view of Sixth Street at Holt Park

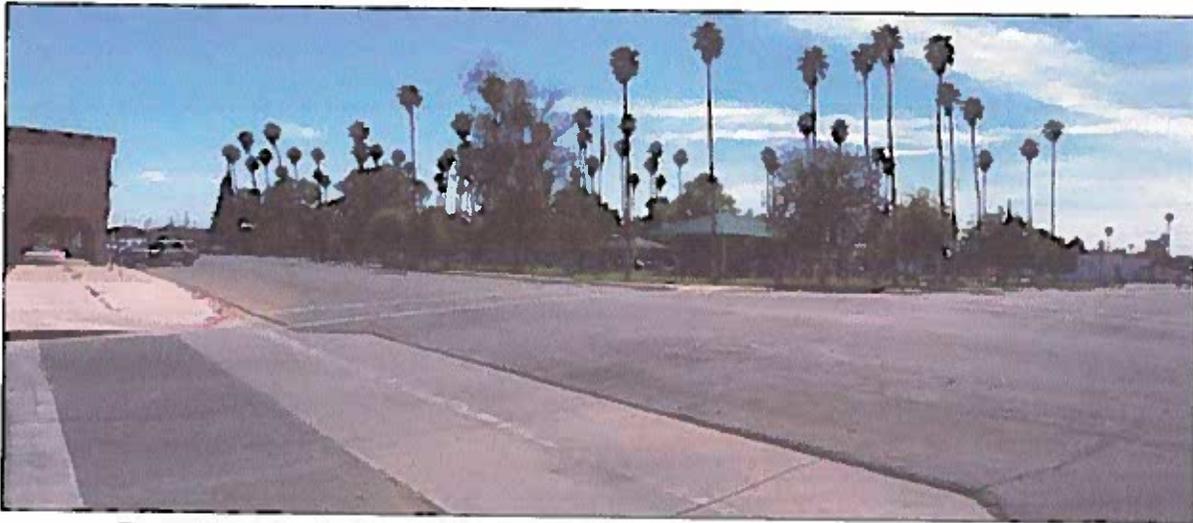


Figure 4-14: Looking southwest toward Holt Park from Sixth Street and Holt Avenue

Recommendation

This recommendation can be completed in phases as funding becomes available. The first phase should be to repaint the crosswalks to a "continental" or "ladder-style" design to increase pedestrian visibility for drivers; see **Figure 4-15**.



Figure 4-15: Phase one of intersection reconfiguration on Fifth Street at Holt Park

The second phase, which can be done in conjunction with the first, would install bulbouts to reduce crossing distances and risks of conflict between drivers and pedestrians as well as install ADA compliant curb ramps. **Figure 4-16** offers various bulbout configurations ranging from paint to concrete. These could be installed in phases.



Figure 4-18: A family and bicyclist use the informal Beale Avenue Extension Pathway between Seventh Street and Fifth Street

Recommendation

This recommendation can also be completed in phases based on funding allocations. The first phase could be a decomposed granite path which would widen the existing path and provide an even surface for all users, as shown in Figure 4-19.



Figure 4-19: Beale Avenue Extension Pathway upon completion of phase one

The second phase involves paving the pathway which offers more even surface for users over time. Paving the pathway will also offer unpaved shoulders for pedestrian or bicyclist overflow. Figure 4-20 shows what this pathway could look like.



Figure 4-20: Beale Avenue Extension Pathway after paving

East Gateway Monument and Crossing

This Plan recommends to improve the eastern entrance to Holtville on Fifth Street to slow down vehicles as they enter Holtville and to provide a sense of arrival.

Existing Conditions

Although the speed limit is posted at 35 miles-per-hour just east of Grape Avenue, the public reported many vehicles traveling at higher speeds while entering from the east and while leaving the city, especially around Grape Street. Grape Street, has an 85-foot crossing distance.



Figure 4-21: Fifth Street entrance to eastern Holtville

Recommendation

The community expressed a desire to have an iconic gateway feature on the east entrance along Fifth Street to complement the one at the west entry. This Plan recommends creating a gateway monument and pedestrian crossing across Fifth Street that would connect the Beale Avenue Extension Pathway to the Orchard View Apartments, a movement that is done repeatedly, especially during school commute. A two-stage crossing is recommended so pedestrians and bicyclists can cross one half at a time and reduce wait time for motorist.

It is recommended that the speed transition from 55 MPH to 35 MPH east of the Orchard View Family Apartments driveway. This relatively new development extended the city limits east from its prior location, generally where the posted 35 MPH sign is located (near Grape Street). Moving the posted speed limit east will enhance safety of all roadway users.

Additional and/or enhanced warning signs should be installed to alert westbound motorists of the speed limit reduction and pedestrian crossing. Examples of electronic speed feedback signs and speed reduction warning signs are used in many communities along SR 395, which is a Caltrans (District 9) owned, operated, and maintained 4-to-5 lane highway that goes through similar sized rural communities. Many of the crosswalks along SR 395 are high visibility with advanced yield lines and transition from 55 MPH to 25 MPH upon entering each city.

In addition, Nevada DOT in the City of Carson on SR 395 introduced raised medians along the roadway, which are not allowed on Caltrans facilities because they are considered a fixed object unless the design speed and 85th percentile speed can be under 35 MPH to allow for Main Street Guideline use and design flexibility. Figure 4-22 shows how these recommendations could offer a potentially safer and more comfortable crossing for bicyclists and pedestrians.



Figure 4-22: The Eastern Gateway crossing could provide a safer crossing for bicyclists and pedestrians as it reduces the crossing distance and provides for two stage crossing.

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The Eastern Gateway can take many forms. Figure 4-23 offers design suggestions.

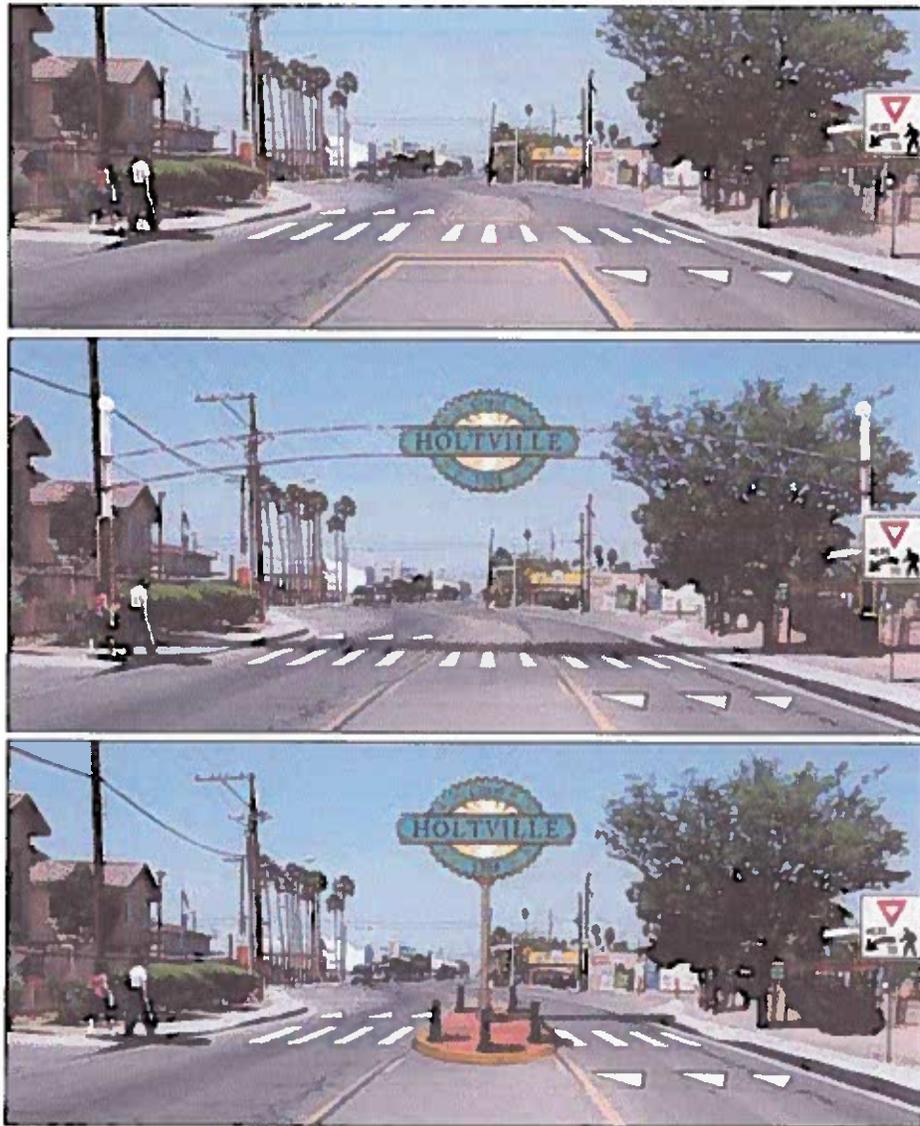


Figure 4-23: Eastern Gateway concepts

Considerations

Alta Planning + Design coordinated closely with Caltrans staff to determine the feasibility of the gateway feature, speed reduction placement, and pedestrian crossing. After multiple meetings, phone conversations, and emails, it was determined that given the change in character proposed through this recommendation, changes to the speed limit zone and other improvements may be allowed by Caltrans; however, final approval would still be required.

Caltrans encourages design flexibility for multimodal projects, noting that, "A 'one-size-fits-all' design philosophy is not Departmental policy."¹ Moreover, Principle 1 of *Main Street, California* (2013), published by Caltrans, grants Flexibility in Design for state highways that are also community main streets.² The California Manual for Setting Speed Limits (2014) and the National Cooperative Highway Research Program Report 737: Design Guidance for High-Speed to Low-Speed Transition Zones for Rural Highways (2012) were provided by Caltrans as references for speed zone transitions.

Although the transition from 55 MPH to 35 MPH at the eastern approach does not meet Caltrans requirements, its staff determined that Caltrans cannot step down (from 55 MPH to 45 MPH to 35 MPH) westbound speeds because a speed zone must be based on 85 percentile speed measurements and be at least a half-mile in length. However, with a change in character as proposed by this Plan, the 35 MPH speed zone could extend east to the Orchard View Family Apartments driveway. This would reduce speeds at the proposed crossing even without a step down. Alta Planning + Design staff respectfully request Caltrans allow additional and/or enhanced warning signs prior to the speed limit reduction to inform motorists of the speed limit reduction. A reduction in vehicle speeds upon entering Holtville will greatly diminish the risk of pedestrian fatality in the event of a collision, as shown in the following figure.

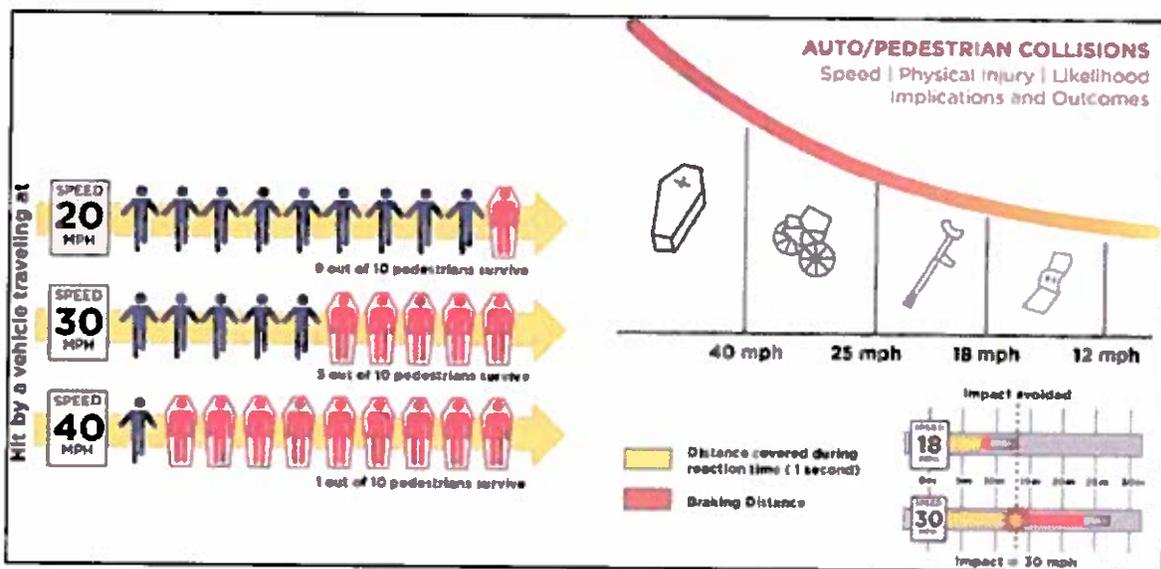


Figure 4-24: Relationship between motor vehicle speed at impact and pedestrian fatality rates

¹ <http://www.dot.ca.gov/hq/oppd/design/2014-4-2-Flexibility-in-Design.pdf>;

<http://www.dot.ca.gov/hq/oppd/design/2014-9-Design-Flexibility-FAQ.pdf>.

² http://www.dot.ca.gov/hq/LandArch/16_livability/main_street/main_street_3rd_edition.pdf.

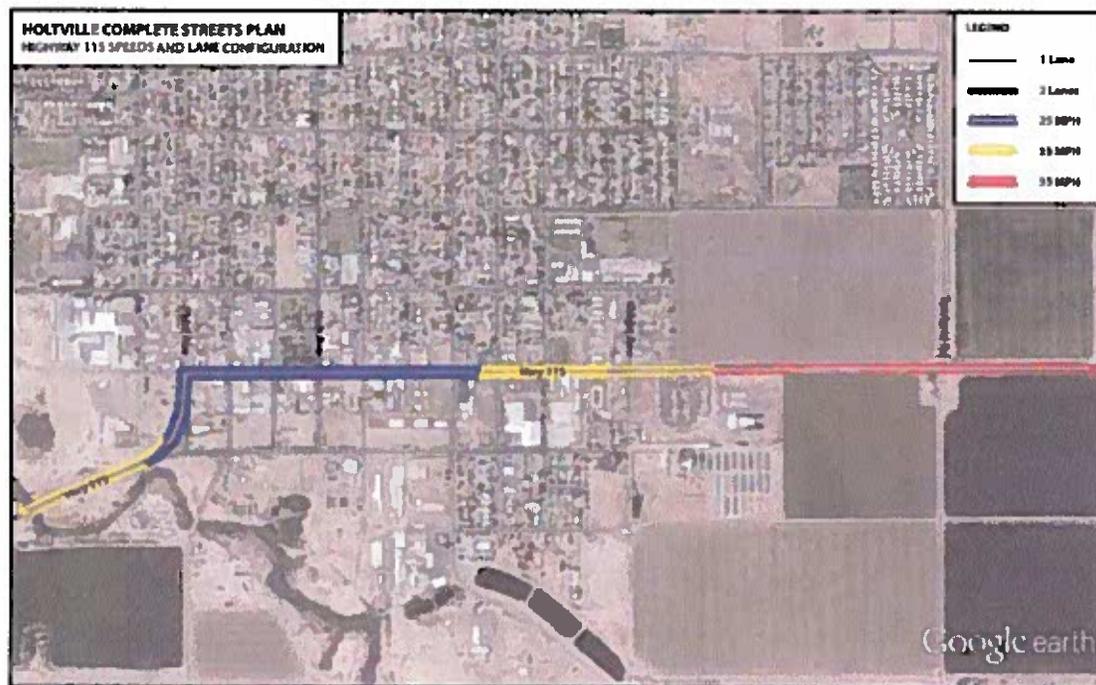


Figure 4-25: Proposed Speed Zones in Holtville

Caltrans Gateway Monument Program

Fifth Street is owned and maintained by Caltrans. Any changes, such as adding a crosswalk and monument, would require Caltrans approval. The Gateway Monument Program became an official Caltrans program in 2008. In this program, Caltrans works with local entities such as cities, counties, or townships to promote enrichment of the cultural and visual environment for transportation system users and local communities by facilitating and coordinating the integration of Gateway Monuments within the operational highway right-of-way, through the encroachment permit process.³ Gateway Monuments are freestanding signs that are not integral or otherwise required for the highway facility that communicate the name of a city, county or township and can include imagery that reflects a community's identity.⁴

Caltrans Gateway Examples

A Gateway Monument is defined by Caltrans as any freestanding structure or sign, non-integral or non-required highway feature that will communicate the name of a Local Entity. A Gateway Monument may include the officially adopted seal or slogan of the Local Entity.⁵

³ http://www.dot.ca.gov/hq/LandArch/gateway/gm_guidelines_final_11-2011.pdf

⁴ http://www.dot.ca.gov/hq/LandArch/index_intro_to_la.htm

⁵ http://www.dot.ca.gov/hq/LandArch/gateway/gm_guidelines_final_11-2011.pdf

Willits, CA

SR 20/Highway 101/Redwood Highway - Mendocino County

The gateway arch spans mid-block over a four-lane roadway with 25 MPH posted speed limit. No crosswalk is present and the neon sign lights up at night with different messages shown depending on the direction traveled. As seen in **Figure 4-25**, the archway supports obstruct the majority of the sidewalk on the eastern side of the roadway.

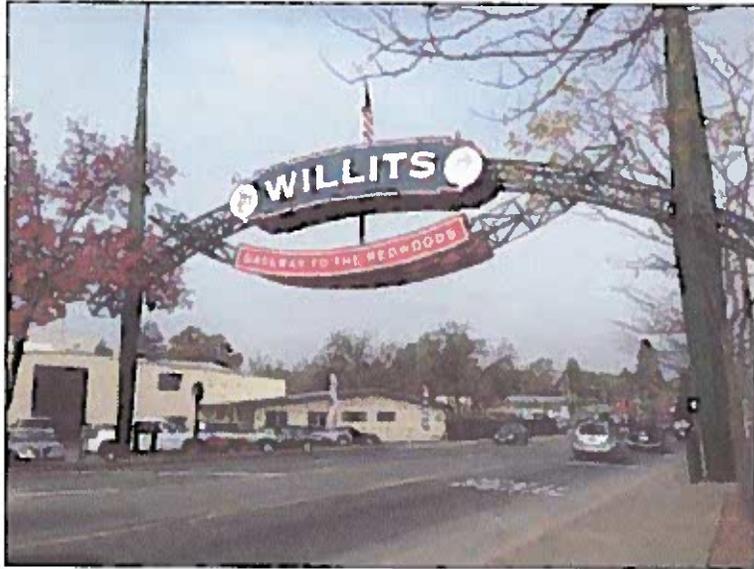


Figure 4-26: Willits archway on SR 20

Dana Point, CA

SR 1/Pacific Coast Highway - Orange County

The gateway arch/pedestrian overcrossing in Dana Point spans an eight lane roadway with a 35 MPH posted speed limit. The bridge was constructed to remove an existing crosswalk to provide pedestrians with safe passage across the busy roadway and new access to Doheny State Beach.⁶ As shown below, the bridge is illuminated at night.

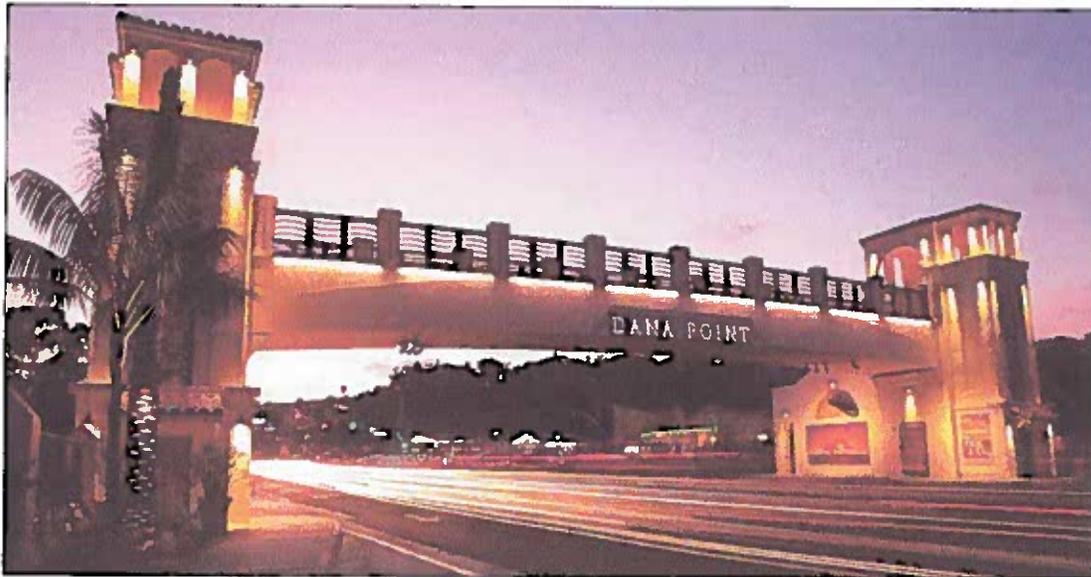


Figure 4-27: Dana Point archway on Highway 1

⁶ <http://www.danapointtimes.com/new-pedestrian-bridge-to-open-may-22/>

Rocklin, CA

Interstate 80 - Placer County

This gateway monument is featured on the central Rocklin Interstate 80 off-ramp. The off-ramp is three lanes with a 45 MPH speed limit.

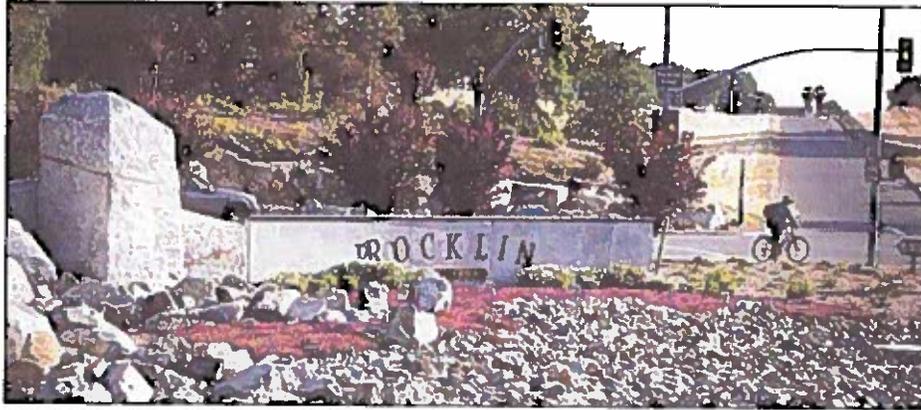


Figure 4-28: Rocklin monument on I-80

Mammoth Lakes, CA

SR 203 - Mono County

Mammoth Lakes installed two gateway signs on either side of the town entrance on Highway 203/Main Street. The road is 35 MPH and 5 lanes wide (two lanes in each direction with a two-way left turn lane). Prior to entering Mammoth Lakes, Highway 203 has a 45 MPH speed limit, which transitions into 35 MPH just prior to the gateway signs.

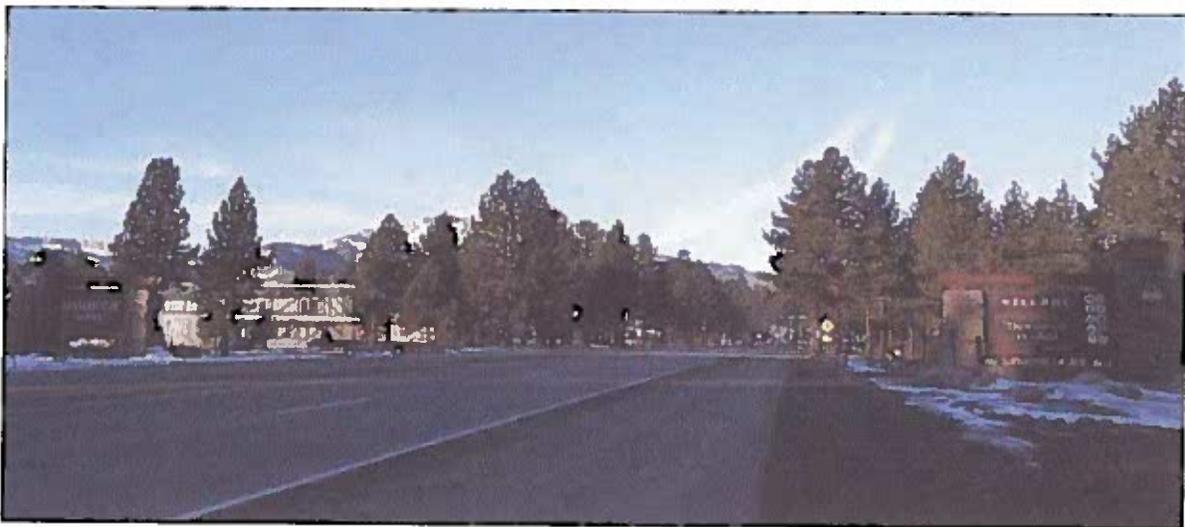


Figure 4-29: Mammoth Lakes signs along SR 203

Crescent City, CA*US 101 - Del Norte County*

Crescent City, CA installed a Hawk Beacon, a high intensity activated crosswalk in early 2015 within the city rather than a monument sign. The crosswalk location was chosen due to the high number of pedestrian crossings along that stretch of US 101/Redwood Highway, a five-lane roadway with a 45 MPH posted speed limit.

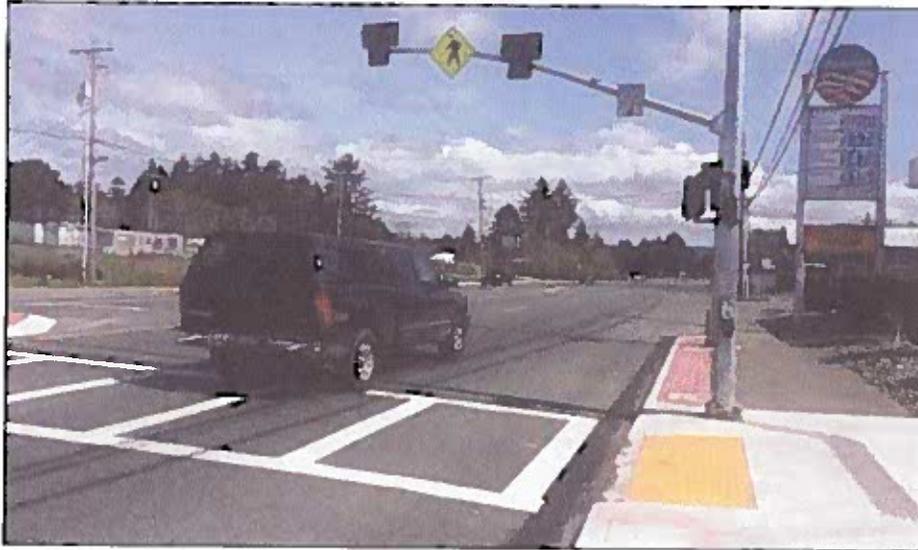


Figure 4-30: HAWK beacon on Highway 101 in Crescent City

Sebastopol, CA*SR 116/Gravenstien Highway - Sonoma County*

SR 116 splits into two-lane, one-way streets through downtown Sebastopol. The posted speed limit is 25 MPH. At several unsignalized T-intersection, bulbouts and flashing signs were added to reduce the crossing distance for pedestrians and to increase driver/pedestrian visibility. Figure 4-31 shows one of these intersections.

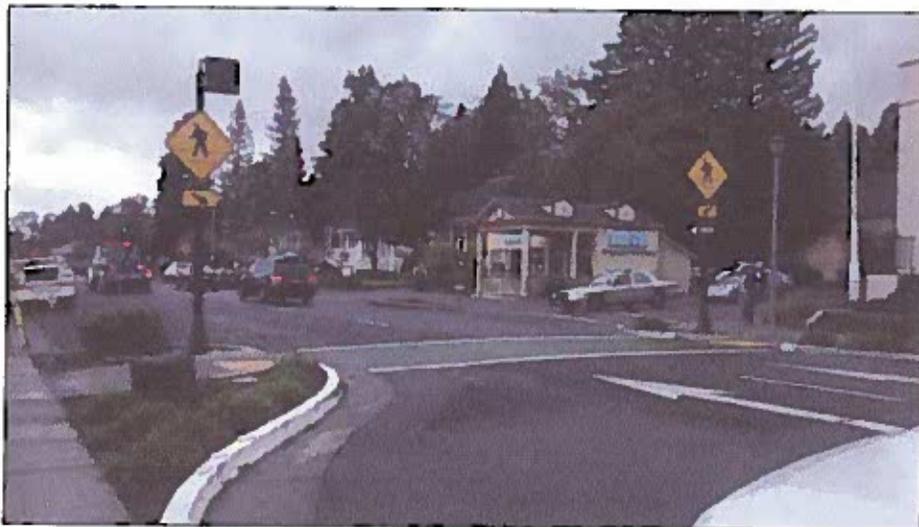


Figure 4-31: High visibility crosswalk on SR 116 in Sebastopol

San Francisco, CA

SR 35/Sloat Boulevard - San Francisco County

State Route 35 through San Francisco was converted from a six-lane roadway to a four-lane roadway with buffered bike lanes in 2012. As a part of the road diet design, mid-block crosswalks were added along with Hawk Beacon crosswalks at unsignalized intersections. The posted speed limit is 40 MPH. Figure 4-32 shows one of continental or "ladder-style" crosswalks installed along this roadway. Figure 4-33 shows a HAWK beacon along a segment of the roadway with buffered bike lanes.



Figure 4-32: Continental crosswalk on SR 35 in San Francisco

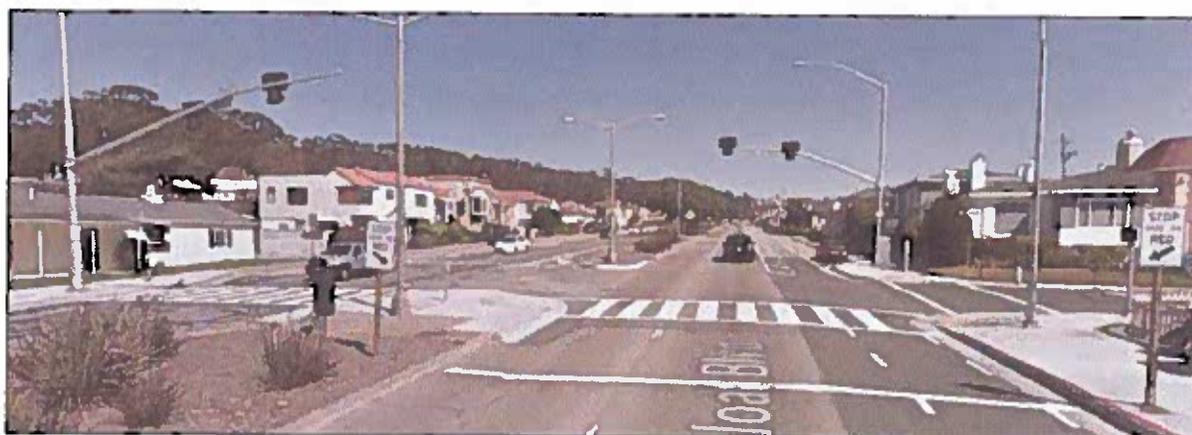


Figure 4-33: HAWK beacon on SR 35 in San Francisco

Gulf Breeze, FL*US Route 98 - Santa Rosa County, Florida*

Although located in Florida, the Gulf Breeze gateway sign is located in a center median of US Route 98, a six-lane roadway with a 35 MPH posted speed limit.



Figure 4-34: Gulf Breeze sign on Route 98

Tucson, AZ*Sixth Avenue - Pima County*

Although not a Caltrans route, Tucson has installed a wider variety of high visibility of pedestrian and bicycle crossings around the city. Sixth Avenue is a four-lane roadway with a 30 MPH posted speed limit. At two unsignalized intersections along Sixth Avenue, Tucson installed a Pelican crossing, a two-stage crossing for pedestrians where a pedestrian uses the crossing by pressing a button to activate the first signal. When the light turns red, a "WALK" signal prompts them to proceed to the median. The pedestrian then walks a short distance along the median to activate the second signal. A second "WALK" indication appears when the traffic signal turns red. The PELICAN uses a standard Red-Yellow-Green signal for motorists and remains green unless activated by a pedestrian. Bicyclists should yield to pedestrians, dismounting if necessary.⁷ Figure 4-35 shows the covered crossing, but at other locations, artwork is displayed along the median.



Figure 4-35: Two-stage Pelican crossing in Tucson, AZ

⁷ <http://www.tucsonaz.gov/tdot/pedestrian-traffic-signal-operation>.

Fifth Street Functionality Improvements

Recommendations so far have touched on several points along Fifth Street, but the whole corridor can be “right-sized” to provide improved functionality for all roadway users.

Existing Conditions

The speed limit along Fifth Street is posted at 25 MPH between Palm Avenue and Maple Avenue. In 2012, average Daily Traffic along Fifth Street was less than 7,000 vehicles. As previously mentioned, there are only six marked crosswalks in the 11 intersections between Palm Avenue and Grape Avenue. Figure 4-35 shows the current roadway conditions and land uses on Fifth Street at Holt Avenue, looking east.

Between 2009 and 2013, 28 reported collisions occurred on Fifth Street, and one of those collisions involved a bicyclist.⁸ According to the Statewide Integrated Traffic Records System (SWITRS) there are three reported collisions within this time window; however, this system was not consistently used until recently and may not accurately reflect all reported collisions. Additionally, the deputy sheriff of Imperial County has noticed and responded to several near misses between motor vehicle drivers and student-age pedestrians.



Figure 4-36: Fifth Street at Holt Avenue looking east

Recommendations

This plan recommends several upgrades along the corridor to make it feel like it is part of a community including installing high visibility crosswalks across all approaches, additional lighting, and stop signs; reducing the number of travel lanes from four to three; replacing parallel parking with angled parking; providing left-turn lanes at intersections (and possible

⁸ Cycle 6 Highway Safety Improvement Program (HSIP) application, Application ID: 11-Holtville-1, Submitted by City of Holtville.

two-way left turn lanes throughout the blocks) for improved predictability and reduced vulnerability; and improving curb ramps. The figures that follow offer examples of how Fifth Street could be rearranged to encourage walking and bicycling, promote economic development, and create a more inviting downtown, as requested through public outreach.

There is a national trend in the reduction of vehicle miles driven. Holtville is no exception. SR 115 has seen dramatic reductions in traffic since the installation of I-8 and has been said to be further declining along the corridor. Even though these numbers are declining, reductions through other means are still required to be able to reallocate vehicle space to other uses without adversely impacting vehicle level of service. Granted the State of California is removing Level of Service as an environmental impact evaluation as part of the California Environmental Quality Act (CEQA).

In general, traffic volumes of 15,000 vehicles per day (vpd) are considered acceptable for a three lane section (two travel lanes with a center turn lane), while traffic volumes between 15,000 and 25,000 vpd are worth analyzing, and over 25,000 vpd typically require 4 or 5 travel lanes.⁹ The AADT along SR 115 is 6,500. This would suggest that the theoretical capacity of this corridor is three times higher than the current volume. As volumes decrease, "road diets" can be used to reallocate underutilized vehicular lane area to other modes, including facilities for cycling, walking, and transit. Other recommended improvements such as increased connectivity, improved pedestrian and bicycle facilities, and better transit service will also reduce volumes.

While the transportation profession often evaluates "daily" volumes to size roadways, historically peak hour volumes are also another consideration to determine the number of lanes. Daily volume thresholds are developed from assumptions based on the percentage of traffic during the day in the peak hour. However, sizing roadways based on one or two peak hours of the day may not be the best utilization of financial resources, create the safest roadway for all roadway users, or allow for the most choices in mobility along the corridor.

To improve safety, connectivity, and accessibility for pedestrians, cyclists, motorists and transit users alike may require a balance of prioritization and how the roadway is designed so the roadway functions and meets the needs of all roadway users for all hours of the day rather than just motorists for one to two hours of the day. For example, if the capacity threshold of a travel lane during the peak hour was 1,500 and for one hour of the day it exceeded that 1,500 by 1 to 100 vehicles but the other 23 hours of the day it was below that threshold, it might be a good policy decision not to widen the roadway because of one hour or a hundred vehicles that could be spread over the adjacent hours. This phenomenon is called "peak hour spreading" whereby excess demand for one hour is served by the adjacent hours. Some congestion and queuing maybe experienced for a short period of time but will dissipate. In addition, since the City of Holtville has a well-designed grid system that is also overdesigned for the vehicle volumes, there is excess capacity on many of the streets.

The excess right-of-way along the roadway could be used for features such as two-way left turn lanes, bike lanes, angled parking, cycle tracks, and other similar features to create a more

⁹ Road Diets Information Guide. Chapter 3.3.5 Average Daily Traffic. FHWA.
http://safety.fhwa.dot.gov/road_diets/info_guide/.

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complete street. Additionally, transforming vacant or underutilized parcels into high-quality development that serves to activate the street and sidewalk will create more vibrant spaces that serve as destinations and economic development generators.

A series of conceptual improvements along Fifth Street follows.

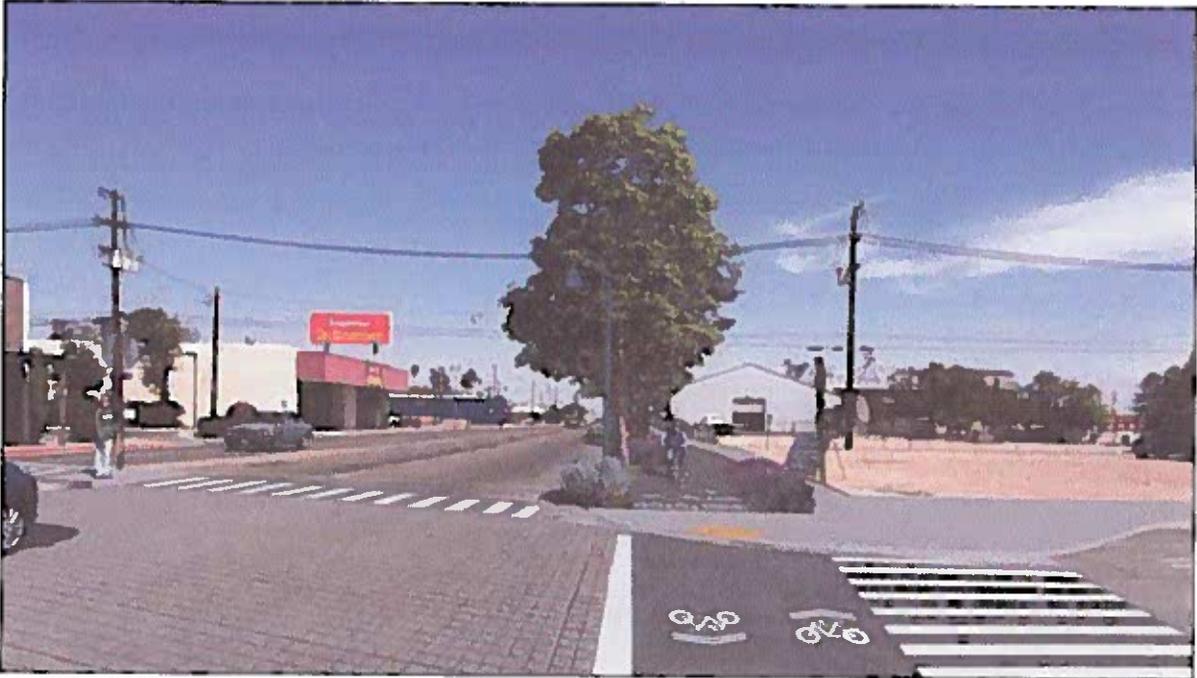


Figure 4-37: Phase one of the Fifth Street reconfiguration could include bulbouts, high visibility crosswalks, bicycle routes, and street trees

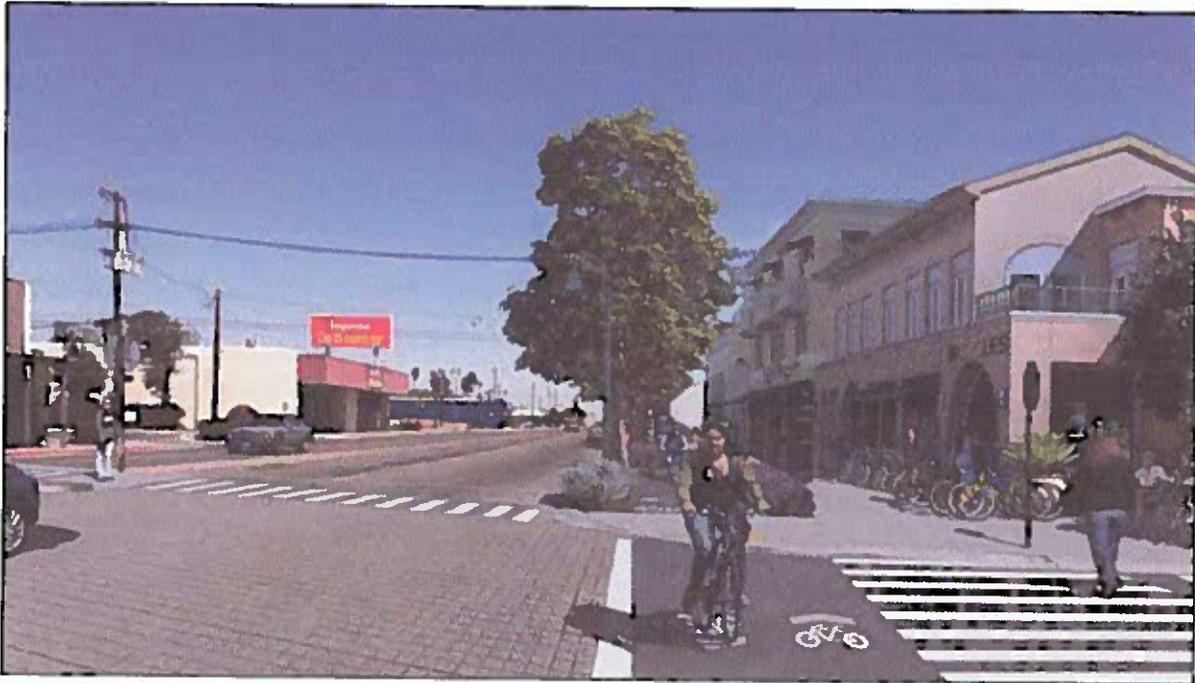


Figure 4-38: Phase two could include a higher variety of land uses

The following image is taken from the Caltrans *Main Street, California "A Guide for Improving Community and Transportation Vitality"* report from 2013. It summarizes why planning streets for people rather than cars provide cities with the best value. When reconfiguring Fifth Street, it is important to keep these ideals at the forefront of the projects.

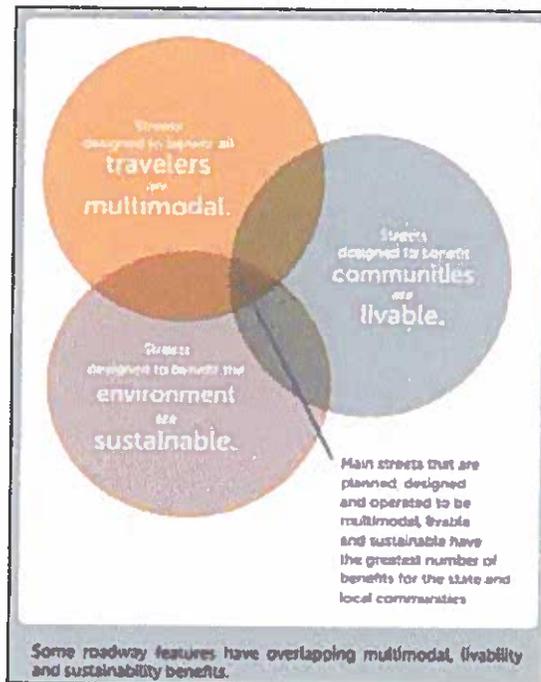


Figure 4-39: Caltrans Main Street report (pg 7) shows benefits to all communities

Seventh Street Reconfiguration

Seventh Street is a major east-west connection that connects Finley Elementary School, Holtville Middle School, and Holtville High School. The wide street is commonly used by students before and after school and could be transformed into a more welcoming street for pedestrians and bicyclists, while also serving to increase shade trees.

Existing Conditions

Seventh Street connects three schools and two parks. It has a posted speed limit of 25 MPH. On average, it is 62 feet across. Since it is greater than 40 feet in width it does not qualify as a residential street as defined by the California Vehicle Code with a prima facie speed of 25 MPH. The generally wide and unstriped nature can encourage speeding and does not provide dedicated space for bicyclists. Parking is generally parallel on at least one side, with multiple blocks having angled parking on one side. **Figure 4-40** shows the current roadway configuration along Seventh Street.

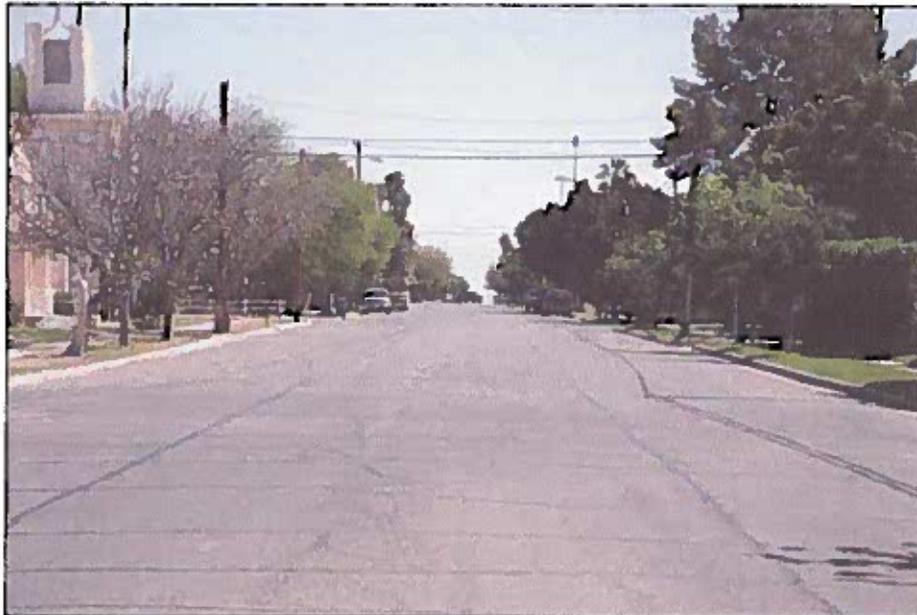


Figure 4-40: Seventh Street at Cedar Avenue looking east

Recommendation

It is recommended to add bike lanes along Seventh Street and to convert the existing parallel parking to angled parking on the north side. This configuration could look one of three ways. **Figure 4-41** shows one option where the bike lanes are located next to the travel lanes.



Figure 4-41: Option one for Seventh Street reconfiguration with bike lanes next to travel lanes

In this scenario, bicyclists could potentially be hit by vehicles in the parking spaces backing up into the bike lane on the north or hit by the doors of parked vehicles on the south side of Seventh Street. Figure 4-42 shows this configuration.¹⁰

Figure 4-43 shows another option for roadway reconfiguration where the bikes lanes are on the outside of the roadway and the parking lanes "protect" the bike lanes from the travel lane on the north side. Caltrans is currently in the process of adopting guidelines for Class IV Separated Bikeways



Figure 4-42: Bicyclist weaves to avoid being hit by parallel parked car

¹⁰ "Door zone open" by VinnyR - Photo of Scott Ehardt taken by VinnyR and released by him into the public domain.

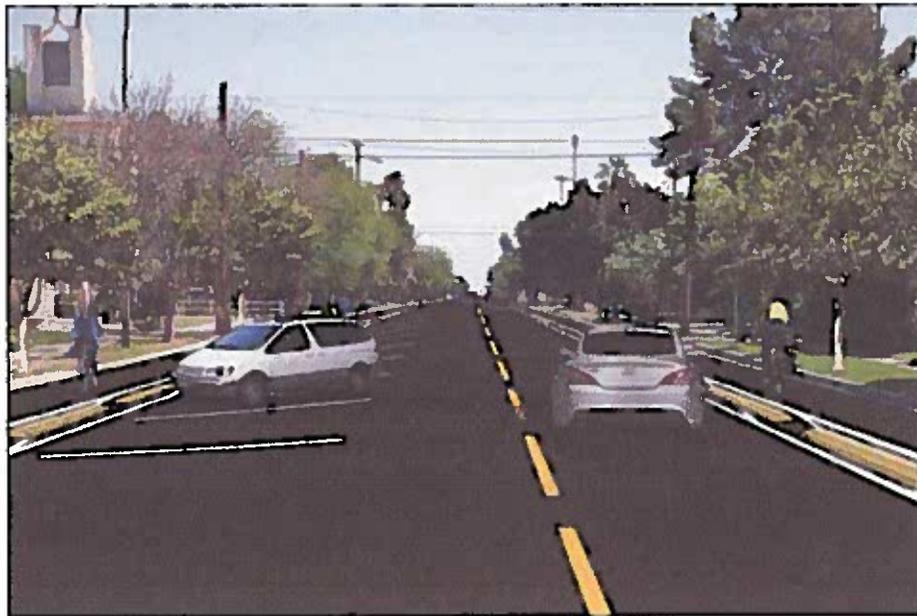


Figure 4-43: Option two for Seventh Street reconfiguration with bike lanes next to the sidewalks

In option two, parking is removed from the south side to allow for a one-way cycle track (Caltrans Class IV Separated Bikeway) to be installed for eastbound bicyclists. The angled parking on the north is shifted south several feet to allow for another one-way cycle track to be installed for westbound bicyclists.

The last option, as shown in Figure 4-44, keeps the angled parking on the north side next to the sidewalk, but removes the parallel parking on the south side and replaces it with a two-way cycle track.



Figure 4-44: Option three would provide a two-way cycle track on the south side of Seventh Street

The cycle track can be protected by other barriers besides concrete curbs such as landscaped planters, bicycle parking, or bollards. For more information about cycle tracks, see **Appendix A: Recommendations Toolkit**.

City Loop

With additional active transportation facilities being installed around Holtville, this Plan recommends to create a bicycle “loop” around Holtville which can serve as a primary bike route. This loop will connect nearly every park, school, and trail in Holtville.

This route can change over time, but it is recommended to follow this route (non-directional):

- Alamo Creek Trail
- Sixth Street
- Myrtle Avenue
- Seventh Street
- Beale Ave Path
- Grape Avenue
- Fourth Street

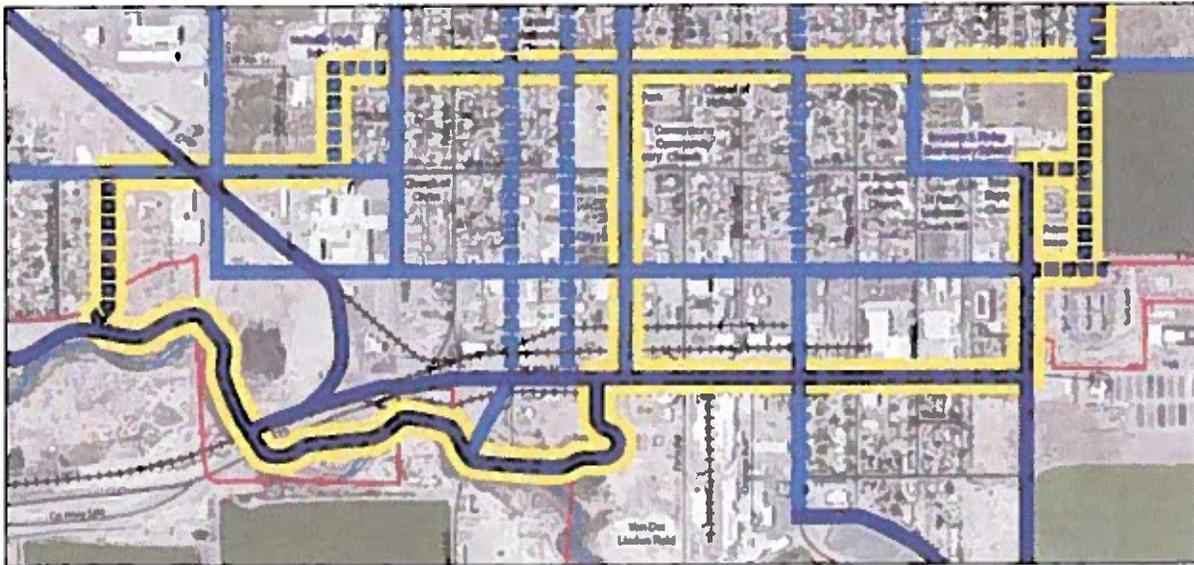


Figure 4-45: Conceptual Holtville City Loop

Holtville Roadway Functionality

This Plan recommends to maximize roadway functionality around all of Holtville. The following list is not exhaustive and can be done in phases. It compliments previous recommendations in this Plan.

- Convert parallel parking to angled parking (phase I)
- Add crosswalks and bulbouts to major intersections (phase I)
- Add benches along areas with high numbers of pedestrians (phase I)
- Add waste receptacles along areas with high numbers of pedestrians (phase I)
- Add street trees (phase II)
- Add bike lanes where feasible (phase II)
- Add/upgrade street lighting (phase II)
- Add public art to beautify the street (phase II)
- Install bicycle parking outside businesses, parks, and churches (phase II)

School Zones

Overall, school zones function relatively well. Finley Elementary showed the highest level of effort to create an efficient and orderly pick-up and drop-off routine. The following recommendations are based on community input, interviews with school staff, and observations at drop-off and/or pick-up times.

Finley Elementary School Recommendations

Cones are placed along the streets to create a one-way flow north on Grape Avenue and west on Sixth Street for drop-off and pick-up times. This creates a major node near the intersection of Sixth Street and Grape Avenue. Crossing guards are located at the intersections of Fifth Street and Sixth Street at Grape Avenue. Additional locations include along Chestnut Avenue and Seventh Street.

Observations and interviews showed high levels of non-conformance with crosswalks along Fifth Street as children go to the Orchard View Family Apartments. Many students use the informal path along the city limit (Beale Avenue extension) or cut through the gas station at the corner of Fifth Street and Grape Avenue. The Plan recommends either moving the existing crosswalk along Grape Avenue at Fifth Street to the east side of the intersection or adding a new crosswalk on the east side of the intersection. It is recommended that intersections should have marked crosswalks at all four approach crossings so as to not cause pedestrians to be further exposed crossing three approach when they could have just crossed one approach. The crosswalk across Sixth Street is on the east and a large number of students come from the Orchard View Family Apartments which is east of Grape Avenue.

Holtville Middle School

The majority of drop-offs and pick-ups occur along Beale Avenue near Eighth Street. A crossing guard is located at this intersection to help students cross Beale Avenue and Eighth Street. Traffic generally flows in both directions, although the northbound movements appears to be higher from observations. School buses use the drop off loop off Beale Avenue at Eighth Street. Cones are place at the entrance to restrict access.

Observations and interviews showed decent numbers of drivers making U-turns along Beale Avenue, issues with drivers parking in parking lots or along the street, and speeding as vehicles leave the area. Interviews concluded that traffic flow could be restricted as is done for Finley Elementary where only one direction would be allowed along Beale Avenue or the intersection with Eighth Street and Beale Avenue would force right turn movements. Additional comments include adding temporary stop signs at the intersection of Beale Avenue and Eighth Street to assist crossing guards and slow traffic. Enforcement/signage to limit longer-term parking, use of parking lots, U-turn movements, and speeding was also identified as a potential improvement.

Holtville High School

The drop-off and pick-up situation appears to be more spread out here than at the other two schools. Seventh Street, Eighth Street, Myrtle Avenue, and Olive Avenue are major access corridors, with a relatively heavy concentration of users along Olive Avenue and at the intersections with Eighth Street and with Seventh Street.

Observations and interviews showed relatively smooth drop-off and pick-up times. However, one-way movement diversion and improvement crossings at the intersections of Olive Avenue at Eighth Street and Seventh Street could improve flow. A deep valley gutter on the south side of the Seventh Street and Olive Avenue intersection tends to slow traffic heading north or south. Improvements along Seventh Street, including bike lanes and shade trees would make this corridor more attractive for walking and bicycling.

Road Rightsizing

Road rightsizing, also known as a road diet or road reallocation, typically occurs with four vehicle lane roadways are reconfigured to a three vehicle lane roadway and bicycle lanes are added, see Figure 4-46.



Figure 4-46: On the left: a four lane roadway. On the right, a five lane roadway, including bicycle lanes

While it may be counterintuitive for some, the figure on the left with four vehicle lanes carry approximately the same number of motorized vehicles as the figure on the right (15,000 average daily traffic). For roadways which see plenty of left turns such as Holtville, the left turn lane provide a space for left turning vehicles to wait for passing vehicles prior to turning instead of waiting in the travel lane and preventing other vehicles from passing, as with the four lane configuration on the left in Figure 4-46. Additionally, the configuration on the right reduces the risk of all types of collisions between roadway users and provides a safer space for bicyclists to use the roadway alongside motor vehicles. This makes roadway rightsizing preferred by local businesses because it allows those without access to a motorized vehicle (2.1 percent of Holtville residents according to 2009-2013 ACS) travel to their businesses more easily.

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Figure 4-47: Clockwise from top left: painted bulbouts; concrete bulbouts solely within City right-of-way; concrete bulbouts solely within City right-of-way with painted bulbouts beyond; and concrete bulbouts

5. Implementation Plan

This chapter provides a strategy for implementing capital project recommendations in this Plan. This implementation strategy and sequence is guided by a ranking system based on a prioritization list developed with the City of Holtville and community members. Phased implementation of the recommended projects and programs will take a significant amount of time, subject to many variables. The most important of these variables include availability of funding for non-motorized transportation. This plan is aimed at increasing the City of Holtville's success in obtaining competitive grant funding and local community and political support.

In the near-term, it is critically important to focus on a group of achievable, high priority projects. Three projects were identified as the highest priority for the community:

- East Gateway Monument and Crossing
- Curb Extensions at Holt Park
- Curb Extensions at Fifth Street and Cedar Avenue

These projects are intended for near-term implementation in the next one to five years. The City's commitment to implementing the mobility goals of the General Plan and commitment to the preparation of the Holtville Complete Streets Plan will attract the wide variety of transportation funding and generate other financing required to complete the remaining project list.

Action Steps

For each recommended project, the following steps should be taken in order to make the project a success.

1. Finalize partnership with Caltrans
2. Secure funding
3. Outreach to community members
4. Gather City Council support
5. Design project
6. Outreach to community members
7. Secure City Council approval
8. Implement project

Prioritization

The three priority projects are listed above. The remaining recommendations from Chapter 4: Recommendations are listed in Table and are based on the estimated time frame for implementation.

Table 5-1: Plan Prioritization

Project	Time Frame
Curb extensions at Cedar Avenue Phase I	Short
Beale Pathway Phase I	Short
Holtville Roadway Functionality Phase I	Short
Curb extensions at Cedar Avenue Phase II	Medium
Holtville Roadway Functionality Phase I	Medium
Elementary School Zone	Medium
Middle School Zone	Medium
High School Zone	Medium
Beale Pathway Phase II	Long
Seventh Street Reconfiguration	Long
City Loop	Long

Cost estimates

Alta Planning + Design prepared cost estimates for projects discussed in the plan on October 5, 2015. These cost estimates represent the best estimates known at that date and represent best planning, engineering, and design practices.

Table 5-2: Curb extensions at Cedar Avenue Phase I

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost
1	Mobilization (10%)	1	LS	\$3,000	\$3,000
2	Traffic Control	1	LS	\$1,000	\$1,000
3	High Visibility Crosswalk	4	EA	\$1,800	\$7,200
4	Painted Curb Extension	4	EA	\$1,500	\$6,000
5	Replace Curb Ramp with New ADA Curb Ramp	3	EA	\$4,000	\$12,000
6	Paint Angled Parking on Cedar Ave (Fourth to Sixth, both sides)	1	LS	\$8,000	\$8,000
7	STOP pavement marking	2	EA	\$200	\$400
				Subtotal Items	\$37,600
				CONSTRUCTION CONTINGENCY	20%
				Total	\$45,100

Table 5-3: Beale Pathway Phase I (Option 1- DG Path)

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost
1	Mobilization (10%)	1	LS	\$14,000	\$14,000
2	Traffic Control	1	LS	\$3,000	\$3,000
3	DG Path (12' wide)	1,250	LF	\$80	\$100,000
4	Concrete Sidewalk with Curb	50	LF	\$140	\$7,000
5	ADA Curb Ramp	1	EA	\$4,000	\$4,000
6	Sign and Post	8	EA	\$500	\$4,000
7	Chain Link Fence	1250	LF	\$20	\$25,000
Subtotal Items					\$157,000
CONSTRUCTION CONTINGENCY				20%	\$31,400
Total					\$188,400

Note: Right of way acquisition/easement for path is not included in this estimate.

Table 5-4: Beale Pathway Phase I (Option 2- AC Path)

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost
1	Mobilization (10%)	1	LS	\$20,000	\$20,000
2	Traffic Control	1	LS	\$3,000	\$3,000
3	AC Path (8' AC path with 2' DG shoulders)	1,250	LF	\$125	\$156,300
4	Concrete Sidewalk with Curb	50	LF	\$140	\$7,000
5	ADA Curb Ramp	1	EA	\$4,000	\$4,000
6	Sign and Post	8	EA	\$500	\$4,000
7	Chain Link Fence	1250	LF	\$20	\$25,000
8	Path Striping	1250	LF	\$1	\$1,300
Subtotal Items					\$220,600
CONSTRUCTION CONTINGENCY				20%	\$44,100
Total					\$264,700

Note: Right of way acquisition/easement for path is not included in this estimate.

Table 5-5: East Gateway and Crossing

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost	
1	Mobilization (10%)	1	LS	\$7,000	\$7,000	
2	Traffic Control	1	LS	\$5,000	\$5,000	
3	Median Island with Concrete Curb	1	EA	\$10,000	\$10,000	
4	RRFB Assembly	4	EA	\$7,500	\$30,000	
5	High Visibility Crosswalk and Yield Line	2	EA	\$2,000	\$4,000	
6	Sign and Post	4	EA	\$500	\$2,000	
7	Roadway Striping	1	LS	\$7,000	\$7,000	
8	ADA Curb Ramp	2	EA	\$4,000	\$8,000	
Subtotal Items					\$73,000	
CONSTRUCTION CONTINGENCY					20%	\$14,600
Total					\$87,600	
Optional Items						
Add Sidewalk with Curb & Gutter along north side					\$60,000 - \$70,000	
Gateway (Spanning roadway)					\$50,000 - 200,000	
Gateway (In Median)					\$25,000 - 50,000	
Gateway (Monument)					\$10,000 - 25,000	

Table 5-6: Curb extensions at Cedar Avenue Phase II

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost	
1	Mobilization (10%)	1	LS	\$10,000	\$10,000	
2	Traffic Control	1	LS	\$5,000	\$5,000	
3	Concrete Curb Extension	4	EA	\$20,000	\$80,000	
4	Sign and Post	4	EA	\$500	\$2,000	
5	Drainage Inlet and pipe connection	2	EA	\$7,500	\$15,000	
Subtotal Items					\$112,000	
CONSTRUCTION CONTINGENCY					20%	\$22,400
Total					\$134,400	

Table 5-7: Seventh Street Reconfiguration (Option 1 - Bike Lanes)

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost
1	Mobilization (10%)	1	LS	\$6,000	\$6,000
2	Traffic Control	1	LS	\$3,000	\$3,000
3	Bike Lane (Striping and Signage)	5000	LF	\$12	\$60,000
Subtotal Items					\$69,000
CONSTRUCTION CONTINGENCY				20%	\$13,800
Total					\$82,800

Table 5-8: Seventh Street Reconfiguration (Option 2 - Protected Bike Lanes)

Item #	Description	Estimated Quantity	Unit	Unit Cost	Cost
1	Mobilization (10%)	1	LS	\$13,000	\$13,000
2	Traffic Control	1	LS	\$6,000	\$6,000
3	Protected Bike Lane (Separation, Striping and Signage)	5000	LF	\$25	\$125,000
Subtotal Items					\$144,000
CONSTRUCTION CONTINGENCY				20%	\$28,800
Total					\$172,800

Table 5-9: Other Planning Level Estimates

Convert parallel to angled parking, one side of road (Short Block 300-foot)	\$1,000
Convert parallel to angled parking, one side of road (Long Block, 600-foot)	\$2,000
Street Tree (in existing planter strip)	\$1,250
Add additional street trees to fill in gaps, one side of road (Short Block - assumed 4 trees)	\$5,000
Add additional street trees to fill in gaps, one side of road (Long Block - assumed 8 trees)	\$10,000
High Visibility Crosswalk and 2 ADA curb ramps	\$10,000
Sign and Post	\$600

Potential Funding Sources

Table 5-10: Potential Funding Sources

Funding Source	Remarks
Federal	
Bus and Bus Facilities Program: State of Good Repair	Can be used for projects to provide access for bicycles to public transportation facilities, to provide shelters and parking facilities for bicycles in or around public transportation facilities, or to install equipment for transporting bicycles on public transportation vehicles.
Bus Livability Initiative	Can be used for bicycle and pedestrian support facilities, such as bicycle parking, bike racks on buses, pedestrian amenities, and educational materials
Federal Transit Act	Typical funded projects have included bike lockers at transit stations and bike parking near major bus stops. FTA funds can also be used for First/Last Mile bicycling and pedestrian improvements within 3 miles of a transit stop. Guideline for the use of 10 percent of the annual CMAQ funds starting in fiscal year 2012-2013 for bike/pedestrian projects through a competitive call to local agencies.
Land and Water Conservation Fund	Federal fund provides matching grants to state and local governments for the acquisition and development of land for outdoor recreation use. Lands acquired through program must be retained in perpetuity for public recreational use. Individual project awards are not available. Recent call deadline was February 2015.
MAP-21 - Surface Transportation Program	A wide variety of bicycle and pedestrian improvements are eligible, including on-street bicycle transportation facilities, off-street trails, sidewalks, crosswalks, bicycle and pedestrian signals, parking, and other ancillary facilities.
MAP-21 - Highway Safety Improvement Program (HSIP)	This program provides funds for the implementation of bicycle transportation facilities that address safety concerns, especially along corridors with high bicycle-involved collision rates. Projects may include education and enforcement programs. The HSIP includes the Railroad-Highway Crossings program.
MAP-21 - Pilot Transit-Oriented Development Planning Program	Provides funding to advance planning efforts that seek to increase access to transit hubs for pedestrian and bicycle traffic.
MAP-21 - Congestion Mitigation and Air Quality Improvement Program (CMAQ)	The amount of CMAQ funds depends on the state's population share and on the degree of air pollution. Recent revisions were made to bring CMAQ in line with the new MAP-21 legislation. There is a broader emphasis on projects that are proven to reduce PM-2.5. Eligible projects include: "Constructing bicycle and pedestrian facilities (paths, bike racks, support facilities, etc.) that are not exclusively recreational and reduce vehicle trips; (and) non-construction outreach related to safe bicycle use." Studies that are part of the project development pipeline (e.g., preliminary engineering) are eligible for funding. "An assessment of the project's expected emission reduction benefits should be completed prior to project selection."

Funding Source	Remarks
National Center for Environmental Health - Health Impact Assessment for Improved Community Design	The grant program aims to increase the capacity of public health departments to include health considerations in transportation and land use planning decisions. The grant provides an average of \$145,000 per year for 3 years to 6 awardees. The grant is generally available every 3 years.
New Opportunities for Bicycle and Pedestrian Infrastructure Financing Act	A proposed bill in Congress to set aside one percent of TIFIA's \$1 billion for bicycle and pedestrian infrastructure projects, such as the conversion of abandoned rail corridors for trails, bicycle signals, and path lighting. For these projects, TIFIA's minimum project cost would be \$2 million. Eligible costs include: planning & feasibility studies, construction, and land acquisition. The bill reserves 25 percent of project funding for low-income communities.
Rivers, Trails, and Conservation Assistance Program	RTCA staff provides technical assistance to communities so they can conserve rivers, preserve open space, and develop trails and greenways.
Transportation Investments Generating Economic Recovery (TIGER) Program	Can be used for innovative, multimodal and multi-jurisdictional transportation projects that promise significant economic and environmental benefits to an entire metropolitan area, a region, or the nation. These include bicycle and pedestrian projects. Project minimum is \$10 million.
U.S. Environmental Protection Agency - Brownfields Program	Assessment grants provide funding for a grant recipient to inventory, characterize, assess, and conduct planning and community involvement related to brownfields sites (locations that have been host to a hazardous substance, pollutant, or contaminant). Revolving Loan Fund (RLF) grants provide funding for a grant recipient to capitalize a revolving loan fund and to provide sub-grants to carry out cleanup activities at brownfield sites. Cleanup grants provide funding for a grant recipient to carry out cleanup activities at brownfield sites.
State of California	
Affordable Housing and Sustainable Communities (AHSC) Program	AHSC grants are available for projects that integrate walking and bicycling improvements with affordable housing developments and transit connectivity. Requirements for housing and transit project components vary based on the frequency of transit in the project vicinity and by the density of the community. The primary criteria for project selection is reduction of greenhouse gas emissions. The 2015 application cycle closed in February and offered approximately \$120 million in grant funding.
Caltrans Active Transportation Program (ATP)	Funds construction, planning, and design of facilities for pedestrians, bicycle riders, and other non-motorized forms of transportation, while also funding non-infrastructure programs related to active transportation. The second application cycle closed in the spring of 2015. The ATP uses MAP-21 federal funds for a portion of the funded projects, so local agencies must adhere to certain federal guidelines.

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Funding Source	Remarks
Clean Water State Revolving Fund Program	The CWSRF program offers low interest financing agreements for water quality projects, which can include "implementation of nonpoint source projects or program." Annually, the program disburses between \$200 and \$300 million. Stormwater management components of bicycle infrastructure projects may be eligible for this funding source. Applications are accepted on a continuous basis.
Climate Ready Grant Program	Climate Ready grants are available for projects located along the coast and coastal watersheds. Shared-use trails are eligible. \$1.5 million total; \$50,000 minimum grant; \$200,000 maximum. Managed by California Coastal Conservancy.
Community Based Transportation Planning Grants	Eligible projects that exemplify livable community concepts including enhancing bicycle and pedestrian access. Administered by Caltrans. \$3 million, each project not to exceed \$300,000.
Environmental Enhancement and Mitigation Program (EEMP)	Funds may be used for land acquisition. Individual grants limited to \$350,000.
Environmental Justice: Context-Sensitive Planning	Funds projects that foster sustainable economies, encourage transit-oriented and mixed use development, and expand transportation choices, including walking and biking. Projects can be design and education, as well as planning. Administered by Caltrans. \$3 million, each grant not to exceed \$250,000.
Habitat Conservation Fund	Provides funds to local entities to protect threatened species, to address wildlife corridors, to create trails, and to provide for nature interpretation programs which bring urban residents into park and wildlife areas. \$2 million available annually. Application deadline is typically in October of each year.
Office of Traffic Safety (OTS) Grant Program	Funds safety improvements to existing bicycle transportation facilities, safety promotions including bicycle helmet giveaways, and studies to improve traffic safety. The grant cycle typically begins with a Request for Proposals in November/December, which are due the following January. For 2015, OTS awarded \$102 million to over 200 agencies.
Petroleum Violation Escrow Account (PVEA)	Funds programs based on public transportation, computerized bus routing and ride sharing, home weatherization, energy assistance and building energy audits, highway and bridge maintenance, and reducing airport user fees.
Public Access Program	Funds the protection and development of public access areas in support of wildlife-oriented uses, including helping to fund construction of ADA trails.
Recreational Trails Program	Administered in California as part of the ATP. \$5.8 million guaranteed set-aside. Managed by the California Department of Parks and Recreation.
Safe Routes to School (SRTS)	In 2014, federal SRTS funds were rolled into the State's ATP to streamline grant allocation. \$24 million combined in ATP for state and federal Safe Routes to School projects for the 2014 cycle. SRTS is primarily a construction program to enhance the safety of pedestrian and bicycle transportation facilities near schools. A small percentage of funds can be used for programmatic improvements. Improvements can be made to target students of all grade levels.

Funding Source	Remarks
Sustainable Communities Planning Grant and Incentives Program	<p>Funded by Prop 84 bond funds, this grant program funds the development and implementation of plans that lead to significant reductions in greenhouse gas emissions, such as rehabilitation of existing infrastructure and the enhancement of recreational resources. The minimum grant award is \$50,000; the maximum award is \$500,000, unless the application is a joint proposal, in which case the maximum award is \$1 million.</p> <p>The 10 percent local match requirement is waived for a proposal that qualifies for the Environmental Justice set-aside.</p>
Watershed Protection Program (Proposition 13)	<p>Grants to municipalities, local agencies, or nonprofit organizations to develop local watershed management plans (maximum \$200,000 per local watershed plan) and/or implement projects (maximum \$5 million per project) consistent with watershed plans. Administered by the Division of Financial Assistance.</p>
Regional and Local	
Clean Air Fund (AB 434/2766 – Vehicle Registration Fee Surcharge)	<p>Administered by San Joaquin Valley Air Pollution Control District. Local jurisdictions and transit agencies can apply. Funds can be used for projects that encourage biking, walking, and/or use of public transit. For bicycle-related projects, funds can be used to install Class I shared-use paths (maximum of \$150,000 per project) or Class II bicycle lanes (maximum of \$100,000 per project). Funds are also available to subsidize transit passes and construct park and ride lots.</p>
TDA Article 3 Funds	<p>Administered by Caltrans. TDA Article 3 funds are allocated annually on a per capita basis to both cities and counties for the planning and construction of bicycle and pedestrian facilities. Local agencies may either draw down these funds or place them on reserve. Agencies must submit a claim form by the end of the fiscal year in which they are allocated. Failure to do so may result in the lapse of these allocations.</p>
Private	
Community Action for a Renewed Environment (CARE)	<p>EPA grant program to help community organize and take action to reduce toxic pollution in its local environment.</p>
Health Foundations	<p>Focus pedestrian improvements for an obesity prevention strategy. Examples include California Wellness Foundation, Kaiser, and the California Endowment.</p>
PeopleForBikes	<p>PeopleForBikes (formerly Bikes Belong) provides grants for up to \$10,000 with a 50 percent match that recipients may use towards the engineering, design, and construction of bike paths, lanes, bridges, and end-of-trip facilities, as well as programs.</p>
Rails to Trails Conservancy	<p>Provides technical assistance for converting abandoned rail corridors to use as multi-use trails.</p>
Surdna Foundation	<p>The Surdna Foundation makes grants to nonprofit organizations in the areas of environment, community revitalization, effective citizenry, the arts, and the nonprofit sector.</p>

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Funding Source	Remarks
Other Private Foundations/ Organizations	Various private foundations and organizations may fund specific components identified in this Plan, such as community encouragement events and other non-infrastructure programs.

6. Recommendations Toolkit

Many of the following recommendations may be used throughout Holtville to meet the goals of this Plan. This toolbox approach will allow for flexibility in design and will provide City staff a set of tools to use when considering improvements along Holtville roadways. Additionally, Caltrans has provided Design Flexibility and NACTO Endorsement Frequently Asked Questions (September 2014), which provides guidance for design flexibility and using designs as provided by the National Association of City Transportation Officials (NACTO).

Phasing and Demonstration Projects

As previously mentioned, project phasing is an effective way to break larger, more costly projects into smaller one that are more attractive for grant opportunities. Additionally, cities often choose to implement demonstration projects to cheaply test concepts and refine design. Demonstration projects, also called tactical urbanism, are low-cost temporary design treatments to test out concepts, such as bike lanes, curb extensions, roundabouts, or public spaces. The City may consider demonstration projects, which may be paired with community events, as funding is available to encourage community feedback.

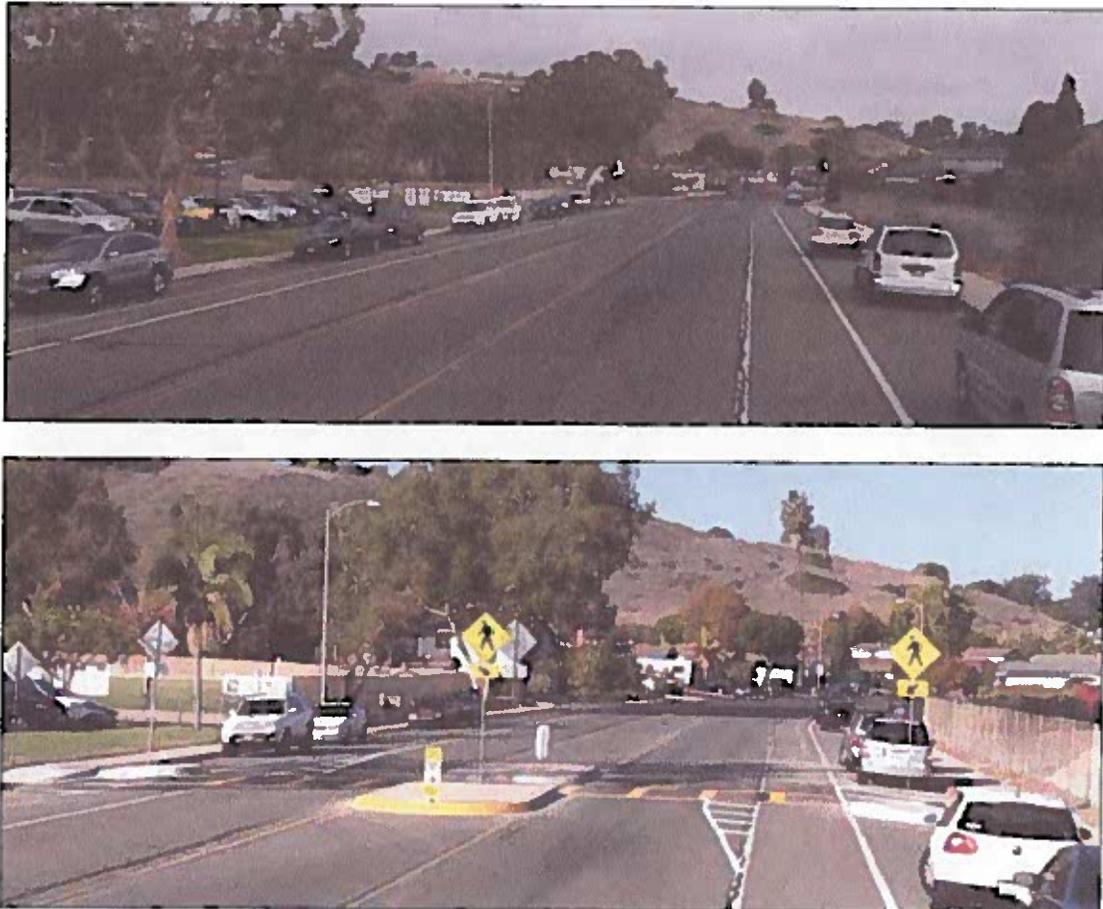


Figure 6-1: Pin-down concrete medians are a lower-cost and quicker solution

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Demonstration projects range from weekend installations to multi-month installations. The following figures are from a recent demonstration project in Morgan Hill, which included both a weekend temporary demonstration testing out various designs and a longer demonstration project that included striping the roadway. Community feedback was critical throughout this process to assess the effectiveness of the changes.

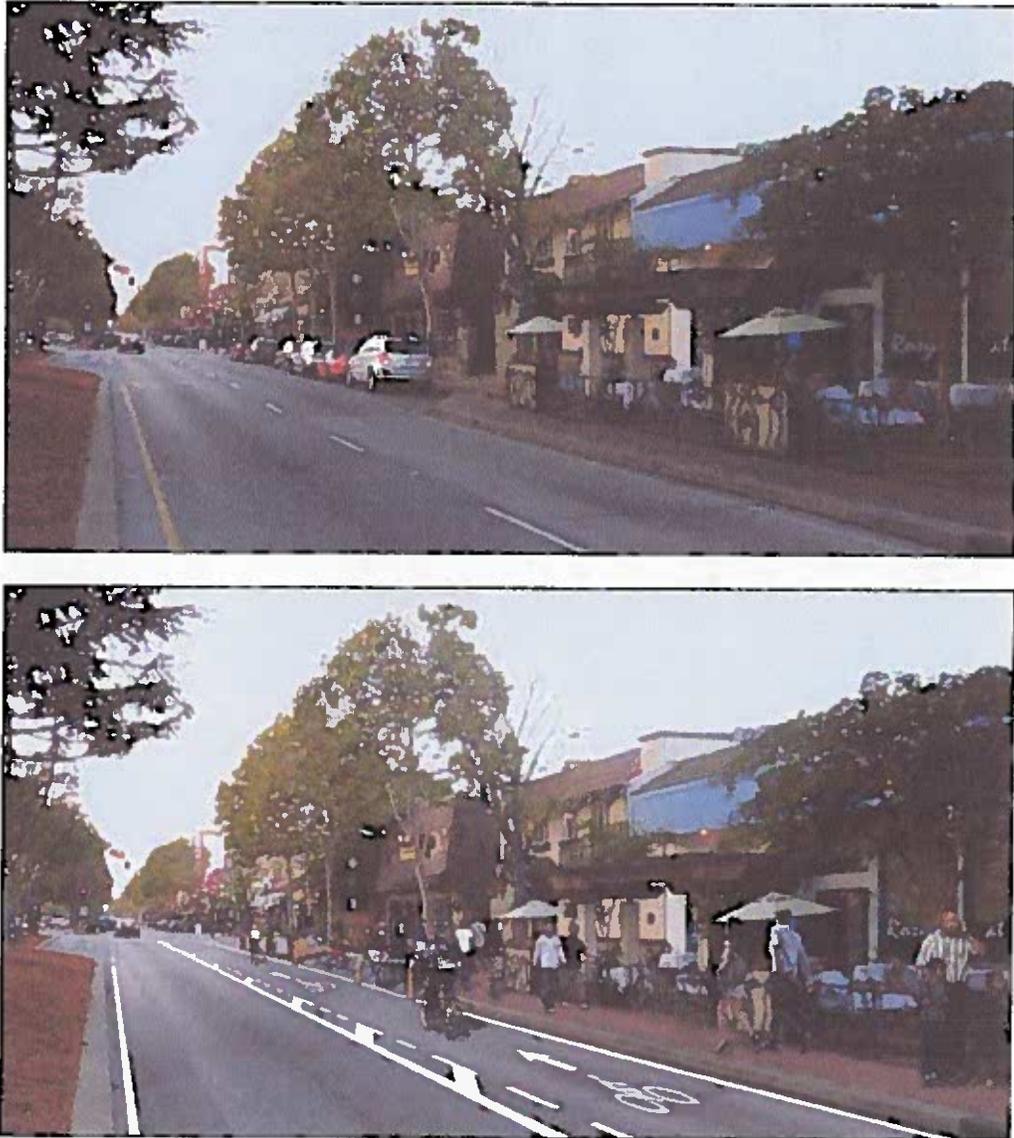


Figure 6-2: Before and after concepts for a demonstration project in Morgan Hill, California

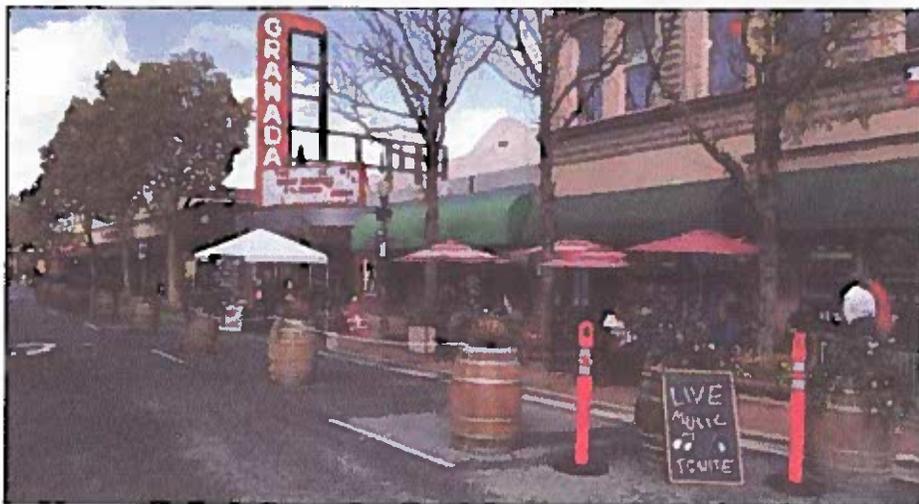
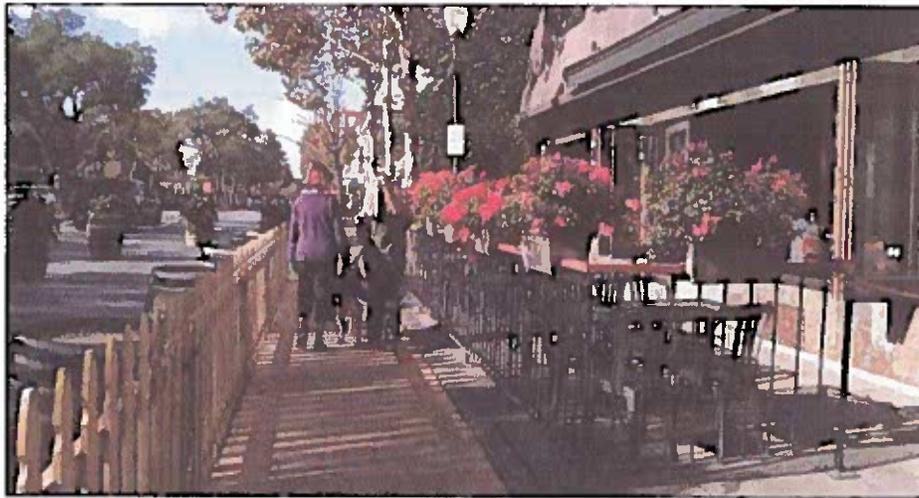


Figure 6-3: Weekend demonstration projects in Morgan Hill, California

Pedestrian Improvements

Curb Extensions

Curb extensions, also called bulbouts or bumpouts, are areas where sidewalks extend into the roadway (usually in the space where a parked vehicle would reside) near intersections. Curb extensions reduce the crossing distance for pedestrians and increase visibility by moving pedestrians further out. Curb extensions have the added benefits of traffic calming, reducing the speed of turning vehicles (a complaint heard by many at public engagement activities), and room for landscaping, benches, or other amenities. New curb ramps should be designed to be ADA and Caltrans compliant.



Figure 6-4: Curb extensions in Colorado

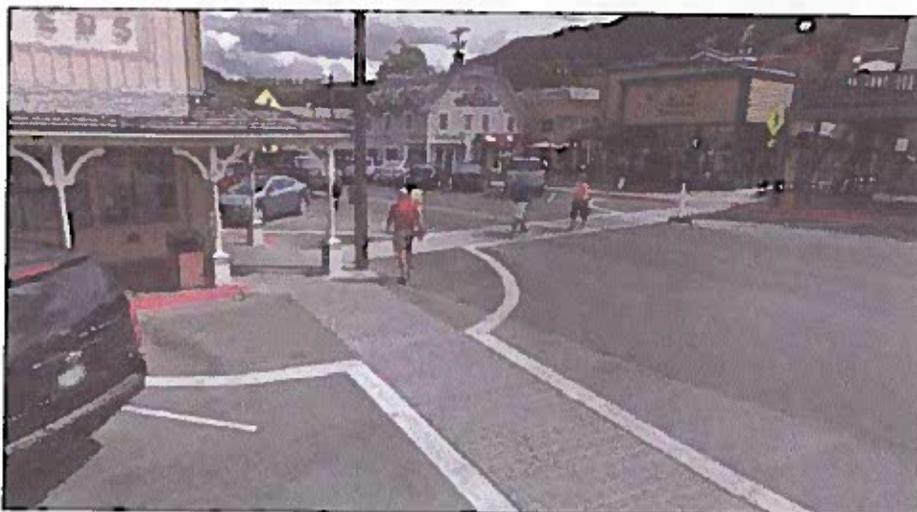


Figure 6-5: Interim curb extensions in Jackson Hole, Wyoming

Curb extensions can often better accommodate the turning radius of vehicles by shifting lanes out and better utilizing the roadway space.

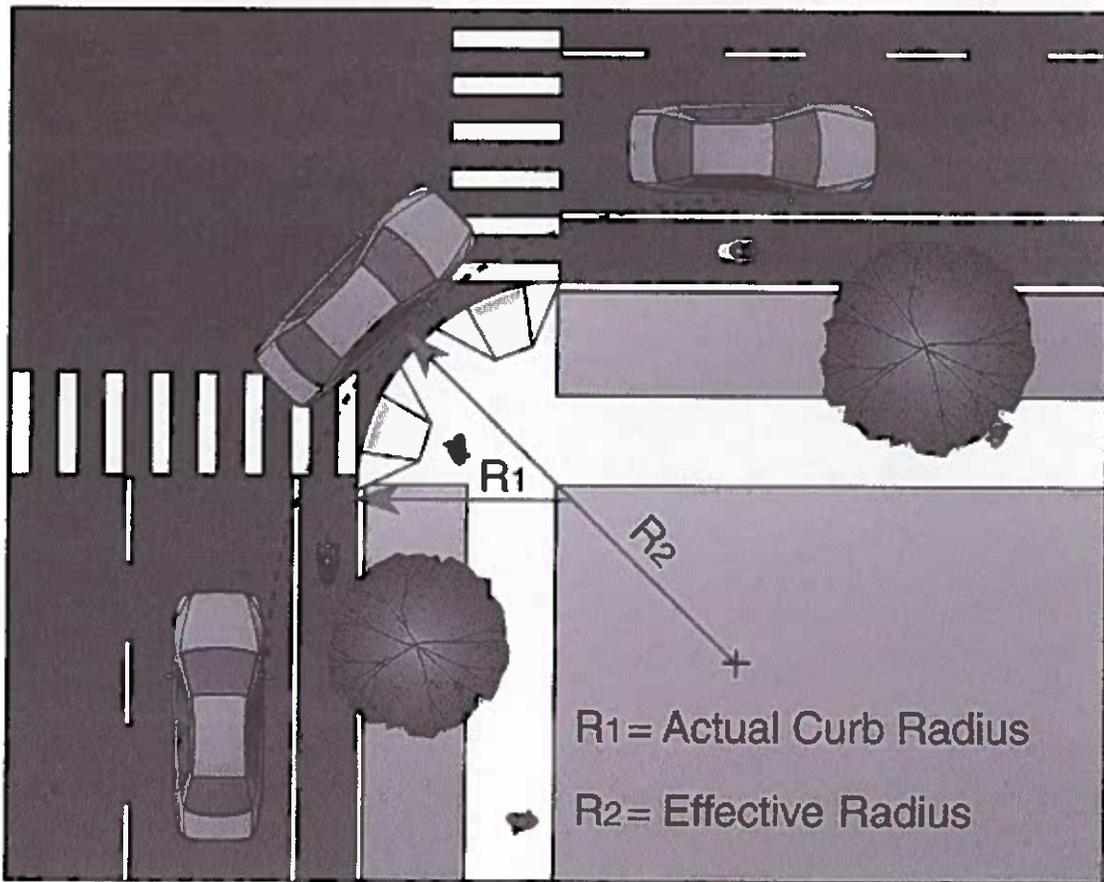


Figure 6-6: Curb extensions and turning radius

Many cities have expressed concerns with bulbouts on truck routes. Curbs designed with tight radii cause truck drivers to encroach into other lanes while turning, which is dangerous for all roadway users; see Figure 6-8. Curbs designed with tight radii and a corner apron (as shown in Figure 6-8) helps to mediate that issue.

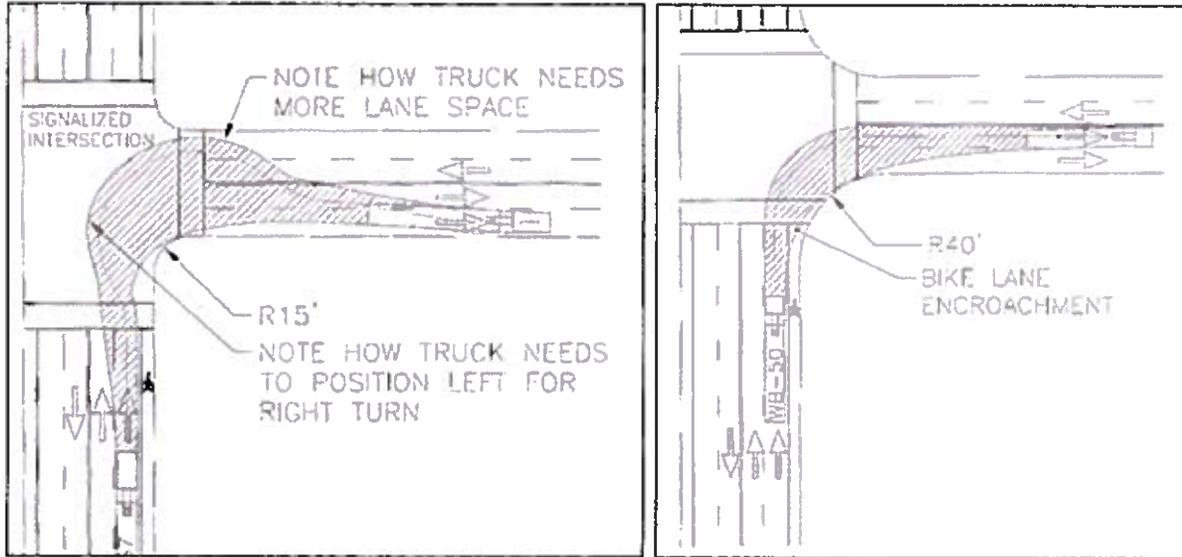


Figure 6-7: It is possible to design and accommodate large trucks through and intersection with a 15' corner radius, but it requires lane encroachment

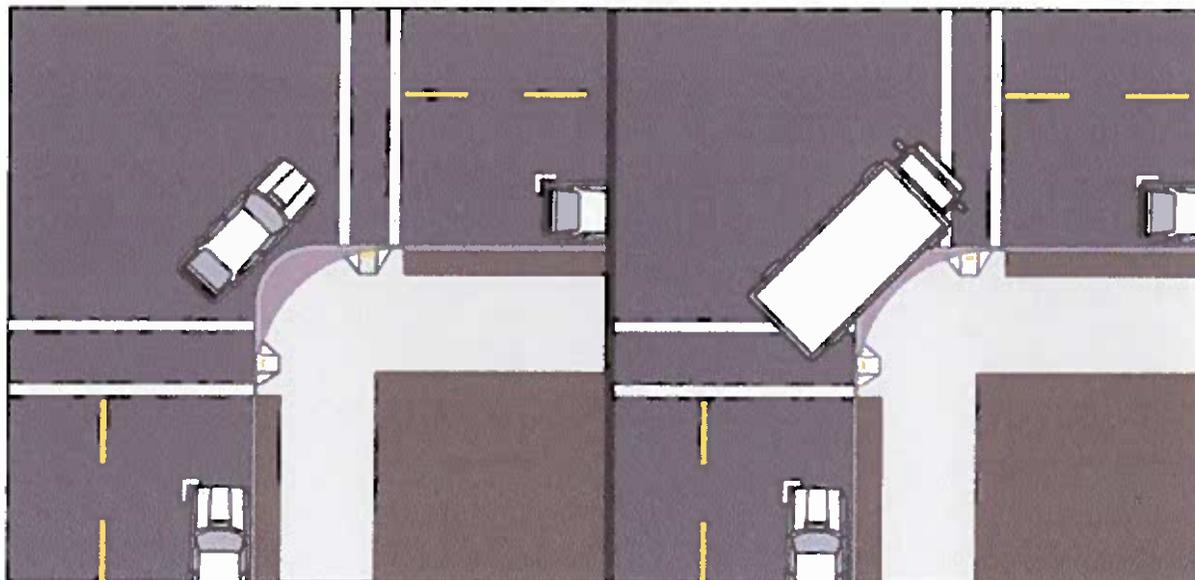


Figure 6-8: A corner apron helps enforce tight turns for standard motorized vehicles while allowing trucks to safely navigate

Crosswalks

Crosswalks come in a variety of designs and can be applied along different roadway types. In general, crosswalks should be kept as compact and direct as possible, facilitating eye contact by moving pedestrians directly into the driver’s field of vision. Sidewalks should be striped as wide as or wider than the sidewalk or walkway it connects to. This will better ensure that when two groups of people meet in the crosswalk, they can comfortably pass one another. High visibility ladder, zebra, and continental crosswalk markings are preferable to standard parallel or dashed pavement markings. These are more visible to approaching vehicles and have been shown to improve yielding behavior.

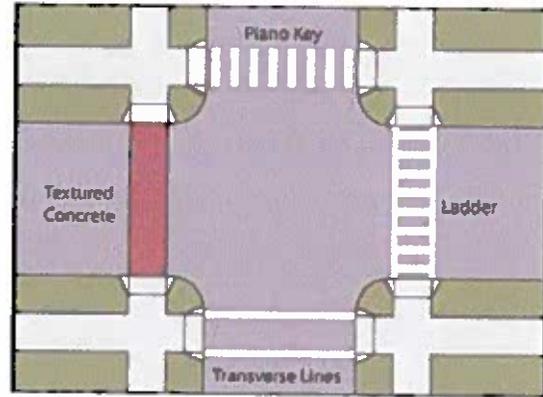


Figure 6-9: Types of crosswalks

Pedestrian crossing facilities range from simple signage and roadway striping to beacons and signals. Figure 6-9 provides a range of facilities and typical applications for each.

PEDESTRIAN CROSSING CONTEXTUAL GUIDANCE <i>At unsignalized locations</i>	Local Streets 15-25 mph			Collector Streets 25-30 mph			Arterial Streets 30-45 mph						
	2 lane	3 lane		2 lane with median refuge	3 lane		2 lane with median refuge	3 lane	4 lane	4 lane with median refuge	5 lane	6 lane	6 lane with median refuge
1 Crosswalk Only (high visibility)	✓	✓		EJ	EJ	X	EJ	EJ	X	X	X	X	X
2 Crosswalk with warning signage and yield lines	EJ	✓		✓	✓	✓	EJ	EJ	EJ	X	X	X	X
3 Active Warning Beacon (RRFB)	X	EJ		✓	✓	✓	✓	✓	✓	X	✓	X	X
4 Hybrid Beacon	X	X		EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓	✓
5 Full Traffic Signal	X	X		EJ	EJ	EJ	EJ	EJ	✓	✓	✓	✓	✓
6 Grade separation	X	X		EJ	EJ	EJ	X	EJ	EJ	✓	✓	✓	✓

LEGEND	
Most Desirable	✓
Engineering Judgement	EJ
Not Recommended	X

Figure 6-10: Pedestrian crossing contextual guidance



Figure 6-11: Continental crosswalk in Oregon

Midblock Crossings

Midblock crossings are crosswalks between intersections and help facilitate pedestrians in areas with heavy midblock activity and areas with larger blocks. Midblock crossings should be located where there is a significant pedestrian desire line, that is a direct line of sight that pedestrians would likely (and often already do) use. Regardless of the paving pattern or material used, the crosswalk should be striped for increased visibility, especially at night.

On wider streets and streets with higher vehicular speeds, medians may be desired to provide designated waiting areas and allow pedestrians to cross one side of the street at a time. This can improve vehicular traffic flow and create a more comfortable experience for pedestrians. As an option, the median may be staggered (preferably to the right) to further encourage pedestrians to make eye contact with motorists and to encourage the two stage movement.

Actuated pedestrian signals (half signals), hybrid beacons, or rapid flash beacons may be considered at greenway crossings, midblock locations, or unsignalized crossings where infrequent crossings make a traffic signal or stop sign unnecessary. Fixed time signals or passive detection are preferable to pushbutton detection.



Figure 6-12: Staggered mid-block crossings help to increase visibility between roadway users

Rectangular Rapid Flashing Beacons

Rectangular Rapid Flashing Beacons (RRFBs) are a type of actuated warning beacons using an irregular flash pattern similar to emergency flashers on police vehicles and can be installed on either two-lane or multi-lane roadways. RRFBs utilize a warning yellow LED stutter flash light bar within the standard crossing sign assembly. These have proven increased motorist yield rates versus other flashing beacon warning systems. These are especially useful at multilane crosswalks. RRFBs may be installed at significantly lower costs compared to traffic signals. These are best used at midblock and other locations where full signals are not warranted.



Figure 6-13 RRFB in median crossing (left) and at sidewalk edge (right)

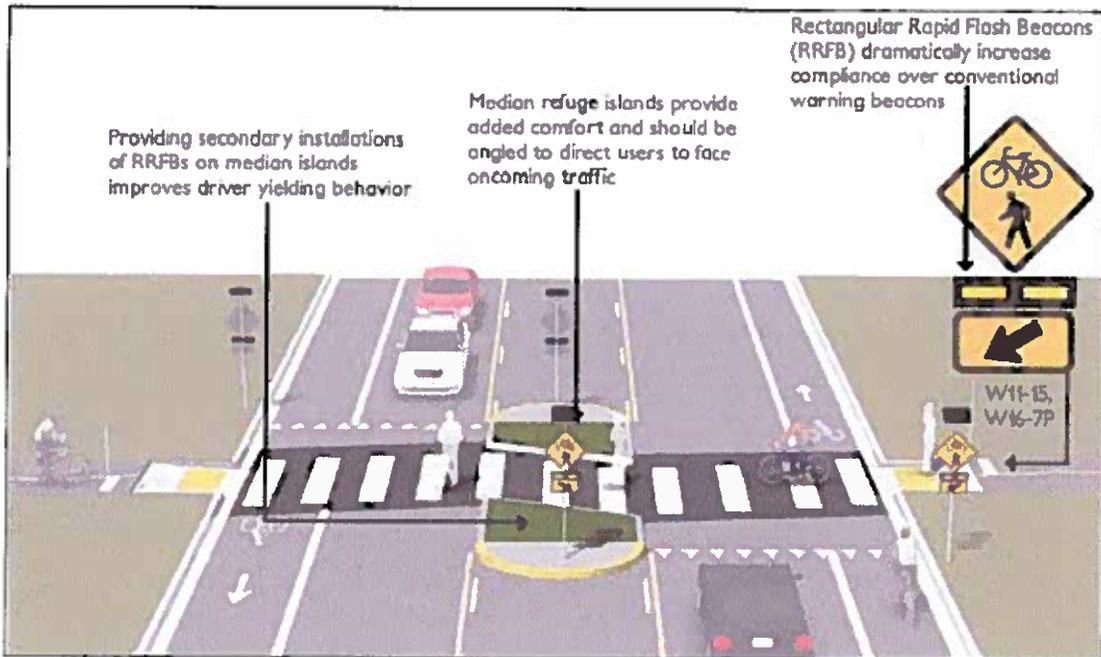


Figure 6-14 RRFB median crossing components

Advance Stop Bar

Advanced stop lines benefit pedestrians, as the pedestrians and drivers have a clearer view and more time to assess each other's intentions when the signal phase changes.¹¹ FHWA does allow for the line to be pushed back from the crosswalk and states, "At signalized pedestrian crossing locations, the vehicle stop line can be moved 15 to 30 feet further back from the pedestrian crossing than the standard four feet distance to improve visibility of through cyclists and crossing pedestrians for motorists (and particularly truck drivers) who are turning right."



Figure 6-15: Advance stop bar at stop-controlled intersection

Advance Yield Markings

Advance yield markings are placed before uncontrolled, mid-block crosswalks to increase yield rates and allow pedestrians to complete a safe crossing.¹² They can be particularly helpful on multilane roads to reduce the potential for a multiple threat crash, which involves a motorist in one lane yielding to allow a pedestrian to cross and the driver in the adjacent lane proceeding into the crosswalk, thus causing a collision. Appropriate locations for advance yield markings are at uncontrolled and midblock crossings, as shown in the following figure.



Figure 6-16: Advance yield markings in Santa Monica (left) and Bishop along SR 395 (right)

¹¹ <https://www.fhwa.dot.gov/publications/research/safety/04091/09.cfm>.

¹² At controlled intersections, advance yield markings should be placed between four and 30 feet back from the intersection. At uncontrolled intersections, they should be placed 20 to 50 feet in advance of the crosswalk.

Bicycle Improvements

Bicycle facilities range from simple signage and roadway striping to physically separated paths. The following figure provides a range of facilities and typical applications for each. The following figure provides illustrations of various crosswalk types.

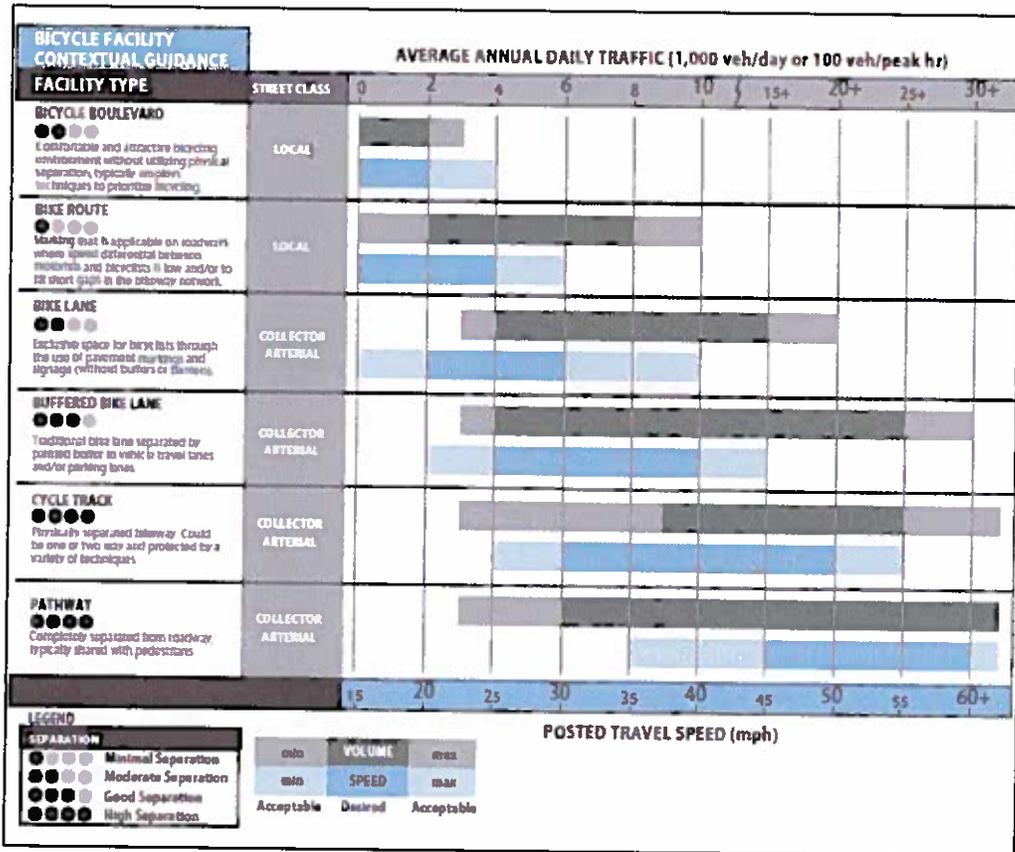


Figure 6-17: Bicycle facility contextual guidance

Bike Corral

Bike corrals typically replace one parking space at the request of a local business or property owner and accommodate 12–24 bikes. Corrals can be installed at corners to increase visibility at an intersection since bicycle parking has no effect on the visibility of pedestrians to moving vehicle traffic. Bike corrals can also be installed in existing red curb areas to reduce impacts on motor vehicle parking.



Figure 6-18: Bike corrals offer more parking than a motorized vehicle parking space

Bike Lanes (Caltrans Class II)

Bike lanes are commonly used on streets with moderate vehicular volumes and provide dedicated space for bicyclists. Bike lanes are typically a minimum of five to six feet wide and may include buffers to provide further separation from moving vehicles in travel lanes and/or parked vehicles. The following figures show various bike lane treatments and associated passing distances provided with each. Buffered bike lanes have the added benefit of encouraging bicycle use to users of a wide variety of abilities and ages.



Figure 6-19: Buffered bike lane for a range of ages and abilities

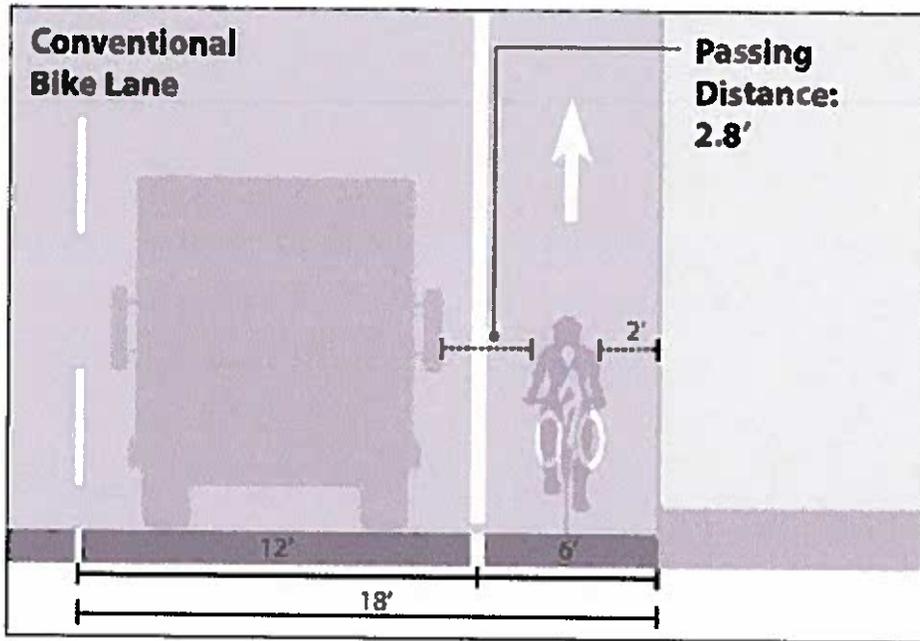


Figure 6-20: Conventional bike lane

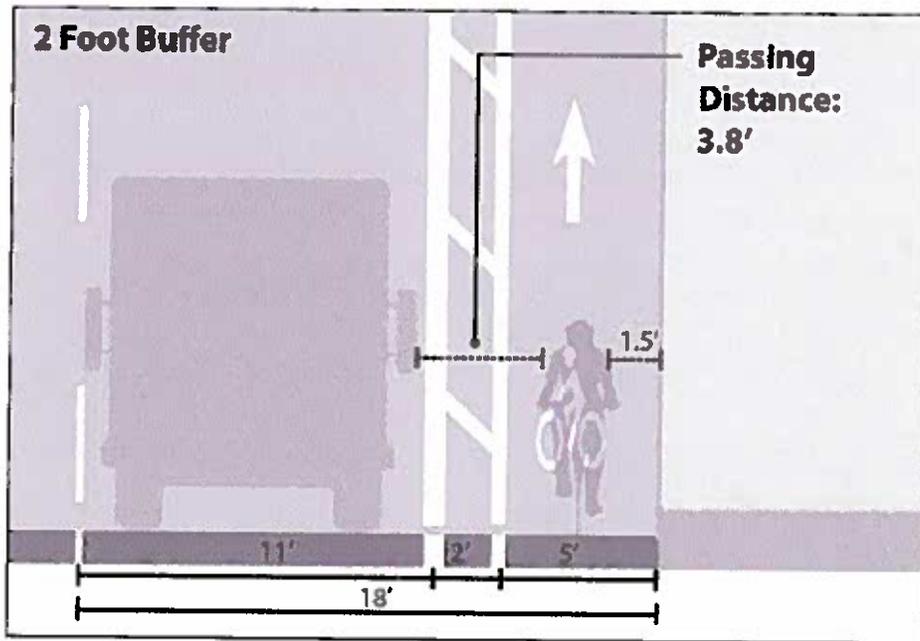


Figure 6-21: Bike Lane with two-foot buffer

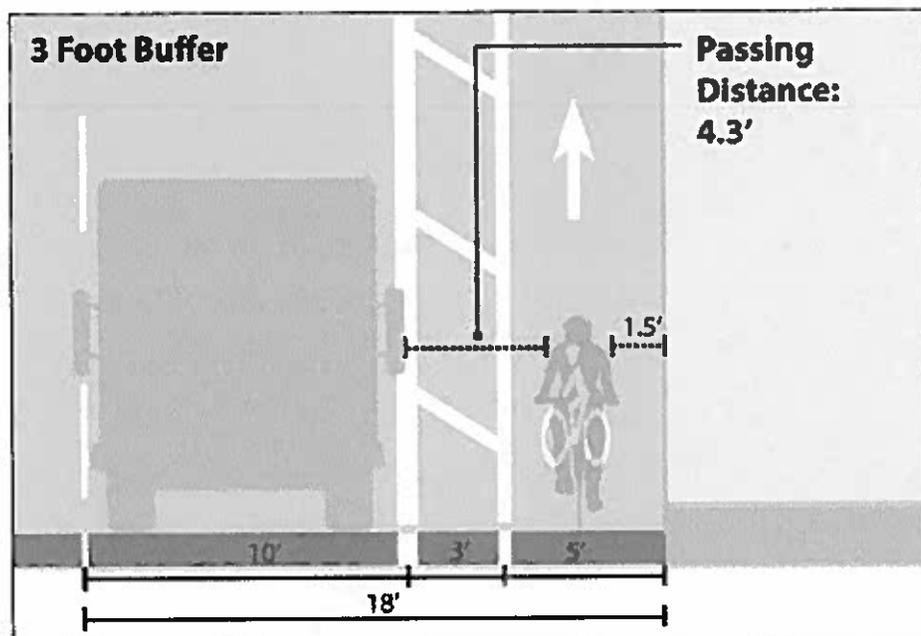


Figure 6-22: Bike lane with three-foot buffer

Separated Bikeways (Caltrans Class IV)

A Separated Bikeway (Caltrans Class IV), also known as a cycle track or separated bike lane is an exclusive bike facility that combines the user experience of a separated path with the on-street infrastructure of a conventional bike lane. A cycle track is physically separated from motor traffic and distinct from the sidewalk. Cycle tracks have different forms but all share common elements—they provide space that is intended to be exclusively or primarily used for bicycles, and are separated from motor vehicle travel lanes, parking lanes, and sidewalks. In situations where on-street parking is allowed cycle tracks are located to the curb-side of the parking (in contrast to bike lanes).

Separated bikeways may be one-way or two-way, and may be at street level, at sidewalk level, or at an intermediate level. If at sidewalk level, a curb or median separates them from motor traffic, while different pavement color/texture separates the cycle track from the sidewalk. If at street level, they can be separated from motor traffic by raised medians, on-street parking, or bollards. By separating cyclists from motor traffic, cycle tracks can offer a higher level of security than bike lanes and are attractive to a wider spectrum of the public.

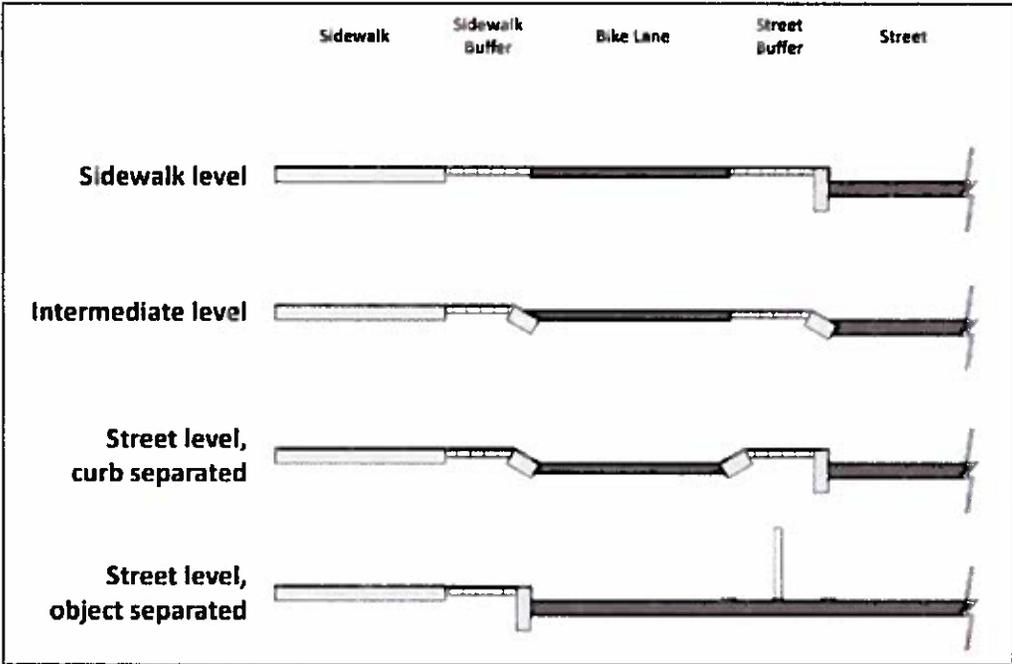


Figure 6-23: Separation shall be at least one of these to discourage the intrusion of motorists into the bikeway and bicyclists from crossing into vehicular traffic lanes. Source: California Bicycle Coalition



Figure 6-24: Separated bicycle lane in Santa Cruz, California

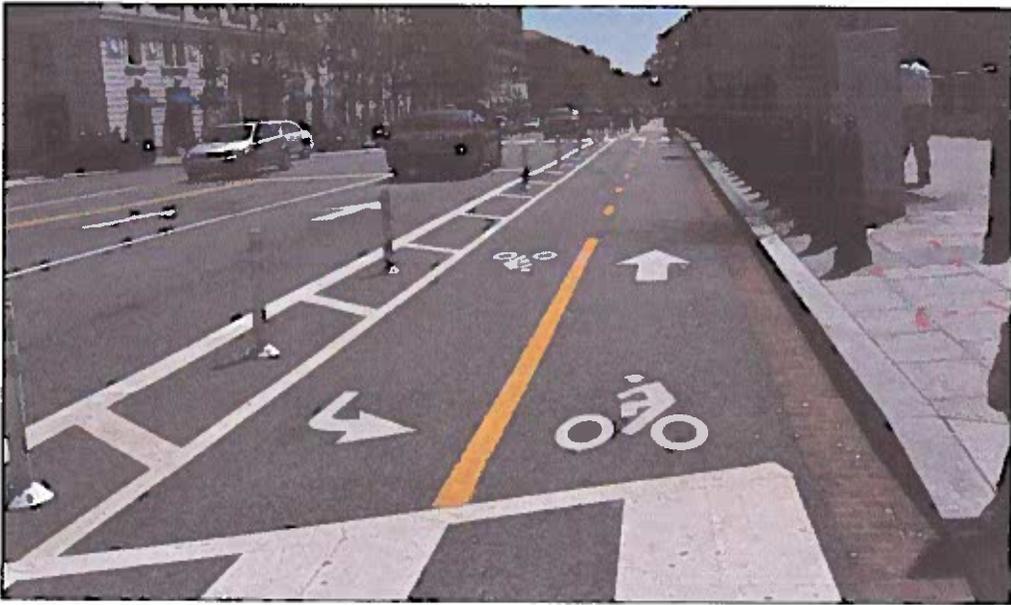


Figure 6-25 Source: <http://77.x-mortgage.com/two-way-cycle-track.php>



Figure 6-26 Source: <https://bikeeastbay.org/cycletrack>



Figure 6-27: Separated bikeway combined with bike corrals

Speed Management

Many of the proposed recommendations and elements in this toolkit are aimed at reducing motor vehicle speeds while also providing additional benefits as previously described. Effective speed management comes from a balance of signage and striping (e.g., crosswalks or speed limit signs), physical improvements (e.g., medians or curb extensions), and enforcement (e.g., sheriff department and crossing guards near schools).

Safety is a major concern for both existing and potential bicyclists and pedestrians. Among all travelers, perceived lack of safety is one of the most frequently cited reasons for not bicycling or walking. Identifying collision sites can draw attention to locations which may be in need of improved safety treatments, particularly if multiple collisions occur at the same location.

With collisions between motor vehicles and pedestrians or bicyclists, the greater the speed of the motor vehicle, the greater the chances become for death or serious injury. The slower drivers move, the greater their awareness is to their surroundings. Additionally, slower to moderate speeds more efficiently move vehicles. The following figures present these concepts.

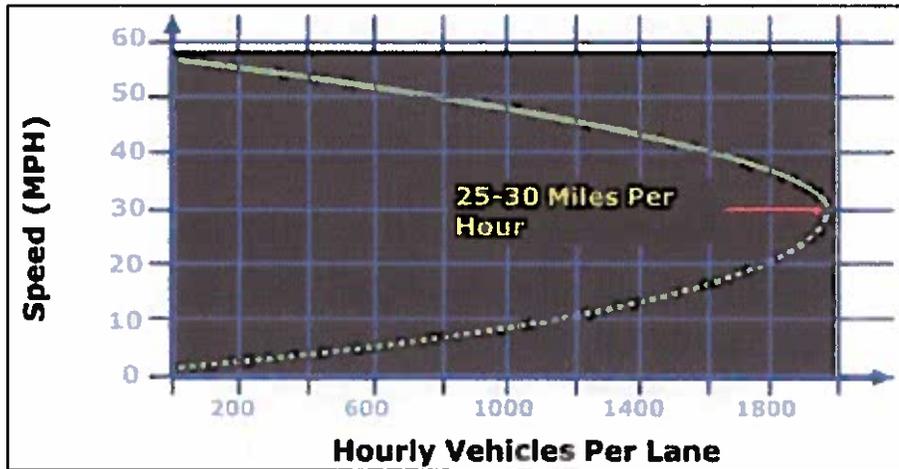


Figure 6-28: Speed to hourly vehicles per lane

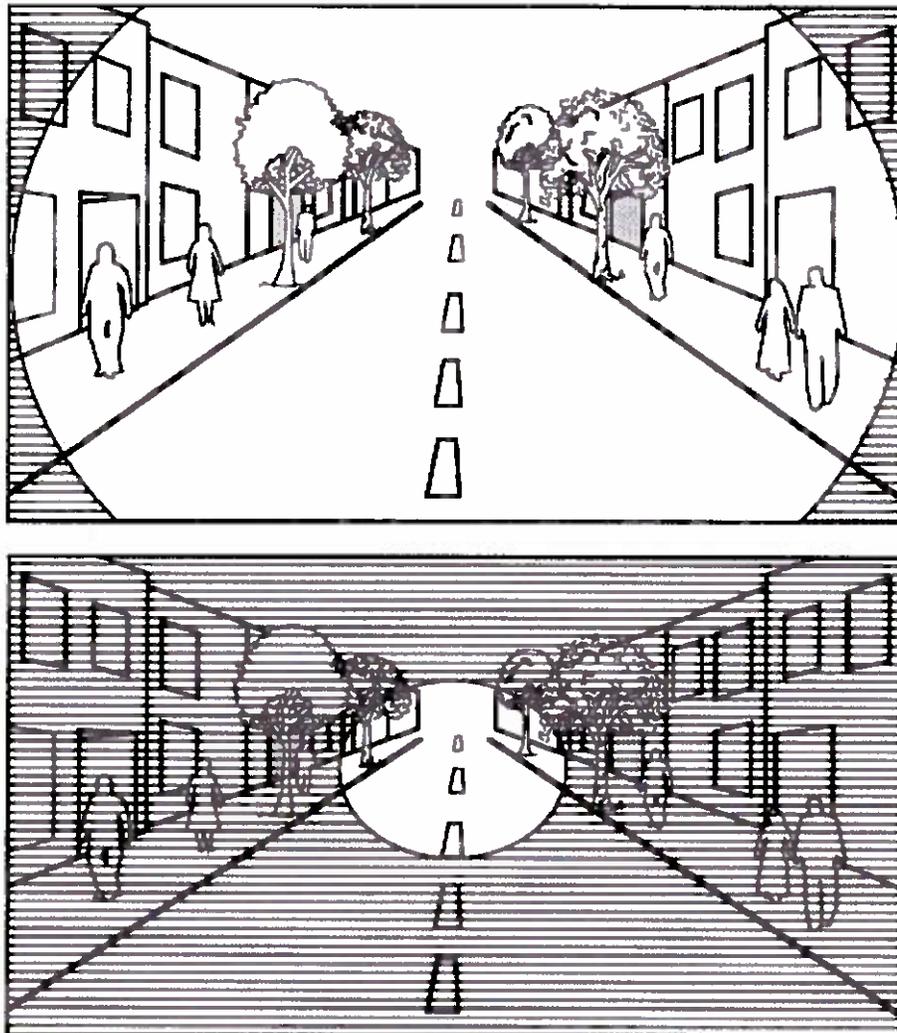


Figure 6-29: Driver's focus at 15 MPH (top) and 30 MPH (bottom)

Speed Feedback Signs

In addition to the previously mentioned tools and recommendations, speed feedback signs have demonstrated improved compliance with posted speed limits. Speed feedback signs may be temporary or permanent and provide motorists with a readout of their current speeds. Often, the text flashes when the current speed exceeds the posted speed limit. These signs may be located near (often before) midblock crosswalks or areas where speeds transition, such as at the east entry into Holtville where speeds go from 55 MPH to 35 MPH. Speed feedback signs may also be used when entering a special area such as a downtown, school zone, or area with high population of seniors or disabled persons.

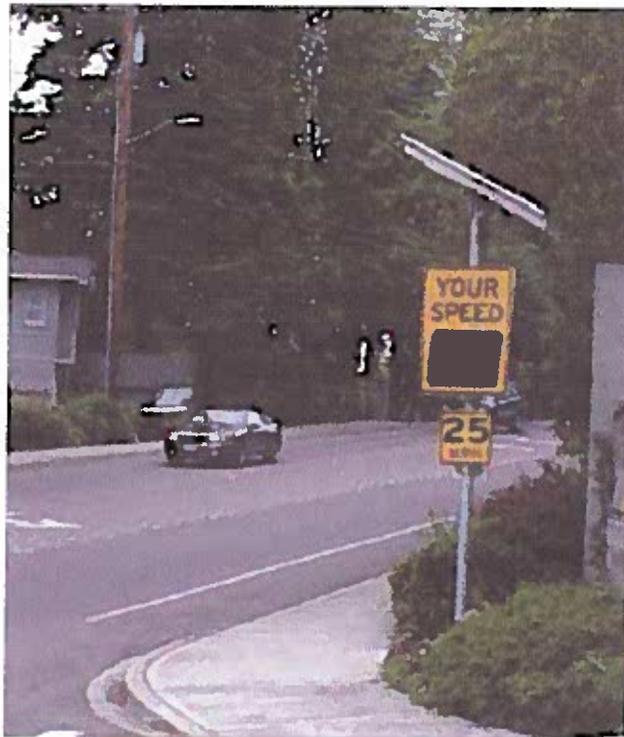


Figure 6-30: Speed feedback signs indicate a motorist's current speed (Source: US Department of Transportation)

City of Holtville

REPORT TO COUNCIL

DATE ISSUED: October 26, 2015
FROM: Nick Wells, City Manager
SUBJECT: City Manager Update

Meeting Date	<u>10/26/15</u>
Item Number	<u>8</u>
City Manager	
Finance	_____
City Attorney	_____

INFORMATION ONLY – NO ACTION REQUIRED AT THIS TIME

WATER ENTERPRISE

Tank Rehab – Corrosion has developed inside the 2.4 million gallon water storage tank that was completed in 2010. Staff obtained multiple bids for repairs and ongoing maintenance, as well as potential solutions to our TTHM issue and potential funding sources. A general estimate was given to rehab the tank at \$225k-275k for the resurfacing and \$350k-400k for the aeration (TTHM solution) portion. The City was recently made aware of the potential for funding from BECC for the tank coating and an application has been submitted for that portion of the necessary repairs. As a condition of the recent renewal of the City's Water Treatment permit, we are now required to submit a Tank Maintenance Program, which will impact this issue. A meeting of City staff was held earlier this month to develop a strategy.

System Controls – Frank Cornejo obtained an updated estimate from TESCO of about \$450k to replace the aged system. Frank had hoped that this project might be put off and the controls might “limp along” for another 18 months while we attack other water issues and staff was able to investigate alternatives and potential funding sources. A recent failure in these controls caused a water break, which has brought this issue back to the “top of the stack” in problems to address. *City staff recently had a conference call with SRF staff regarding funding this issue. We are following up and will be reporting to Council soon.*

Floating Solar – A contract was signed with Infratech Industries for the installation of a floating solar array on the City Water Ponds for the purpose of producing power for the Water Treatment facility. Five plan iterations have been recently submitted to the Building Department and forwarded to the City Engineer for review with continued issues on the electrical drawings. A conference was held last month at DD&E (Infratech's consultant) involving the project's electrical engineer, the City Engineer, several IID staffers and City Manager to address questions regarding the electrical drawings. It is believed that the remaining issues have been clarified and *revised plans are expected to be submitted this week.*

SEWER ENTERPRISE

WWTP Project – The \$15 million project to rehab the City's Wastewater Treatment Plant, financed through the State Clean Water Revolving Fund along with BECC and the EPA through NADBank, is finally under construction. *Weekly meetings have not produced any major issues.*

TRANSPORTATION PROJECTS

Fifth Street & Grape Avenue Pedestrian Roadway Improvement Project – This project essentially adds or improves curb, gutter and sidewalk on the north side of Fifth Street at Grape Avenue,

using CMAQ funding coming through ICTC. Development Design & Engineering, Inc. was selected as the RE/CM and Pyramid Construction as the construction contractor. The pre-construction conference was held a few weeks ago and the Caltrans permits have finally been procured. Work is expected to begin in the next few weeks after Caltrans submittal reviews.

Sixth Street Pavement Improvements – In response to informal discussions with City Council, resurfacing and pavement improvements have been proposed on Sixth Street from Holt Avenue to Melon Avenue if funding becomes available. An application to ICTC for RSTP funding for this project in response to the recent Caltrans Call for Projects has been submitted. ***RSTP applications will be reviewed by the ICTC TAC committee along with other area project on 11/19 to score and rank the projects for recommendations on funding.***

Ninth Street Pear Canal Undergrounding to Olive Avenue Project – Over the past several years the Pear Canal dirt ditch along the north side of Ninth Street has all but been eliminated. The last project section from Cedar to Palm left only a small length of a couple hundred feet to Olive Avenue. Beyond that point, the canal is concrete lined. An application to ICTC for both RSTP and CMAQ funding for this project in response to the recent Caltrans Call for Projects has been submitted. ***Last week the CMAQ portion of this project passed the Technical Advisory Committee scoring portion with a recommendation for funding. RSTP applications will be reviewed by that committee on 11/19 and it is hoped that the project grades high enough for funding that portion as well.***

Cedar Avenue Sidewalk Improvements Project – ***Improvements to curb, gutter and sidewalk along the east side of Cedar Avenue between Fourth and Fifth Streets were approved through the CMAQ program. An RFP for a design engineer is currently being developed.***

Walnut Avenue South Improvements Project – Proposed improvements consist of installation of AC pavement overlay and widening to 50' in width along the length of Walnut Avenue from Fourth to First Street. The ICTC scoring process resulted in the project being awarded 82% of the funds requested through RSTP in the total amount of \$498,000.

Environmental documents and a Federal ID Packet were submitted to Caltrans in April, 2015, with a finance number assigned that month. Administration staff is in the process of clearing right-of-way issues and has contacted all utility agencies. It is anticipated that a Caltrans ROW Certification will be obtained no later than next month and procurement can begin.

Complete Streets Project – A team of consultants visited Holtville several months ago to work on this project funded by an Environmental Justice Grant through the Department of Transportation. The purpose of the project is to review and recommend improvement to all forms of mobility in the City. A variety of workshops and community input led to several interesting preliminary recommendations, so we are awaiting their final report. They have been discussing the possibility of getting Caltrans to allow for amenities to help slow traffic as it enters town from the east on 115. They feel as if they have gotten some traction with Caltrans on a level over the District 11 management, which they were anxious to include in a report to the City that they now expect to finalize soon. ***A draft report was provided for staff and Council review.***

PUBLIC WORKS

PARKS

Pete Mellinger Alamo River Trail - The fourth phase of this project, which included trail improvements and bridge installations between the Holtville Skate Park and Earl Walker Park, is basically complete. The City Council recently voted to rename this project the "Pete Mellinger Alamo River Trail." A revision to the planned trail head sign was prepared and will be produced, which will complete this phase of the project.

The City received notice through the State Parks Department that additional funding had become available for, among other things, pedestrian trails. The Holt Group submitted an application and although there was not enough additional funding awarded to fund the amenities that were cut from the last phase of the project, the City was awarded a sizeable sum to offset some past project overruns (i.e. the bridge installation).

BMX Park - City staff began a few months ago to lay out a basic BMX track in the dirt pit adjacent to the new Skate Park. At this point, it will be a pretty simple “bare bones” facility, but it is hoped that grant funds can be found to develop it in coming years. Work has been temporarily halted until the weather is a bit more hospitable and proper signage is procured to avoid any liability issues.

ADMINISTRATION

BUILDING PERMITS - The City has issued **96** building permits during 2015.

Holtville Estates - Eight homes have been sold and occupied. They have begun work on the third phase of 7 additional homes. There is enough interest already - with 3 homes currently under contract in Phases III and 3 lots on hold in Phase IV - that they currently plan to stay in construction through the completion of the subdivision.

Holtville Luxury Apartments – A project has been in the works for some time at the corner of Ninth and Melon, just outside the City limits. I recently met once again with John Hawk, who represents the landowners, about the project. As the Service Area Plan has now been adopted, work will once again begin with the City and LAFCO to propose annexation and development.

OTHER

Sustainable Communities General Plan Grant - A contractor to administer the grant was approved by Council in May. We worked with them to find qualified consultants through an RFP for proposals and a preferred firm was selected by the committee. A contract was approved with Michael Baker International and a conference call for was held a few weeks ago to kick off activities.

Army Transcontinental Convoy – *The Convoy passed through Holtville on Friday, 10/16. Unfortunately, they were unable to stop, but a good crowd of folks came out to Holt Park to wave and cheer the procession on to their next stop in El Centro.*

Black Dog Trestle Fire Matter – As everyone is well aware, the lawsuit trial over the fire started by Black Dog that damaged the old railroad trestle recently concluded. Although the \$665,588 was less than we sought, it is considerably more than as offered to settle. We will soon begin the process of “Bridging” the funding gap that remains to get the structure repaired.

MEETINGS & EVENTS RECENTLY ATTENDED:

- 10/12/15 Management Staff Meeting *City Hall*
- 10/14/15 ICTC Management Committee Meeting *Holtville City Hall*
- 10/14/15 CCMA Meeting *Holtville City Hall*
- 10/14/15 FFA Opening/Closing Ceremonies Contest *Holtville High School*
- 10/15/15 Holtville Farmers Market *Holt Park*
- 10/16/15 Veteran's Day Parade Committee *City Hall*
- 10/16/15 Army Transcontinental Convoy Procession *Main Street*
- 10/19/15 Management Staff Meeting *City Hall*
- 10/20/15 LTA Fund Audit *City Hall*
- 10/20/15 Health Insurance Meeting w/ David West *City Hall*
- 10/22/15 CMAQ Project Scoring *ICTC Offices (EC)*
- 10/22/15 IVECA Board Meeting *EC ED Offices*
- 10/22/15 Meeting w/ Ralph Cordova *re: Various CoH Issues/Opportunities* *City of EC*
- 10/23/15 Veteran's Day Parade Committee *City Hall*

UPCOMING EVENTS:

- 10/26/15 Management Staff Meeting *City Hall*
- 10/27/15 Emergency Operations Training *City Hall*
- 10/29/15 IC League of Cities Dinner *Asagio Restaurant (Brawley)*
- 10/29/15 Meeting with CalRecycle Representatives *City Hall*
- 10/30/15 Veteran's Day Parade Committee *City Hall*
- 11/06/15 Veteran's Day Parade Committee *City Hall*
- 11/09/15 Holtville City Council Meeting *City Hall*
- 11/11/15 2nd Annual Veteran's Day Parade *Fifth Street/Holt Park*
- 11/16/15 Holtville Planning Commission Meeting *City Hall*
- 11/16/15 Auditors Onsite *City Hall*
- 11/19/15 RSTP Project Scoring *ICTC Offices (EC)*
- 11/19/15 IVECA Board Meeting *EC ED Offices*
- 11/19/15 IVEDC Annual Dinner *Casa Blanca (Bornt Home-Holtville)*
- 11/21/15 Green & Gold Hall of Fame Gala *Barbara Worth Country Club*
- TBD Briefing on ACO / Emergency Comm Survey *IC Admin Offices*

If you have any questions about any of the information presented, please feel free to contact me directly.

Respectfully submitted,



Nicholas D. Wells
(760) 356-4574

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MEETING DATE:	10-26-15
APPROVED FOR AGENDA	<i>[Signature]</i>
CITY MANAGER	_____
FINANCE MANAGER	_____
CITY ATTORNEY	_____

City of Holtville

REPORT TO COUNCIL

DATE ISSUED: October 13, 2015
FROM: Alex Silva, Fire Chief
SUBJECT: Monthly Report for September 2015

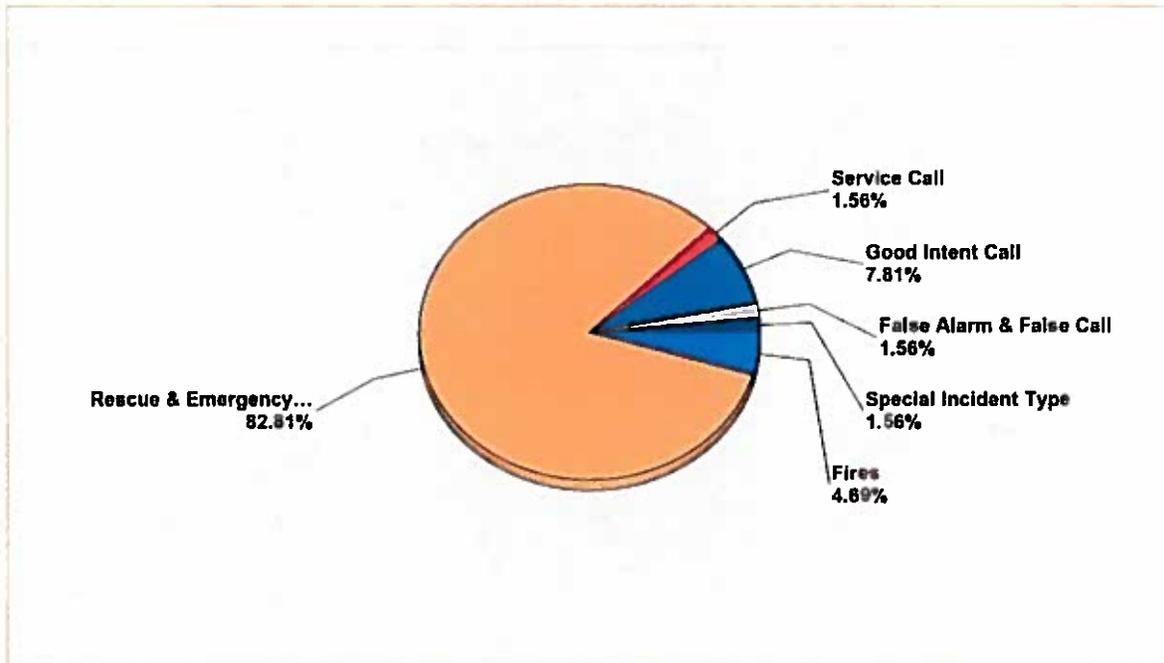
THIS IS INFORMATION PROVIDED TO THE CITY COUNCIL. NO ACTION IS REQUIRED OF THE CITY COUNCIL.

During the month of September, our fire crew came back from strike team in Butte County. On Friday, September 11, 2015 we participated along with our Holtville Based Sheriffs Deputies at Finley School to honor Patriots Day. This year we were joined by the students and fire crew from Holtville Alabama. Thanks to our Mayor Ward, City Council member Jim Predmore and our City Manager in participating in our event. The following is the monthly report for the month of September 2015.

Emergency Calls	64
Training hours	145
Residential inspections	18
Commercial inspections	8

Cordially submitted

Alex Silva
Fire Chief



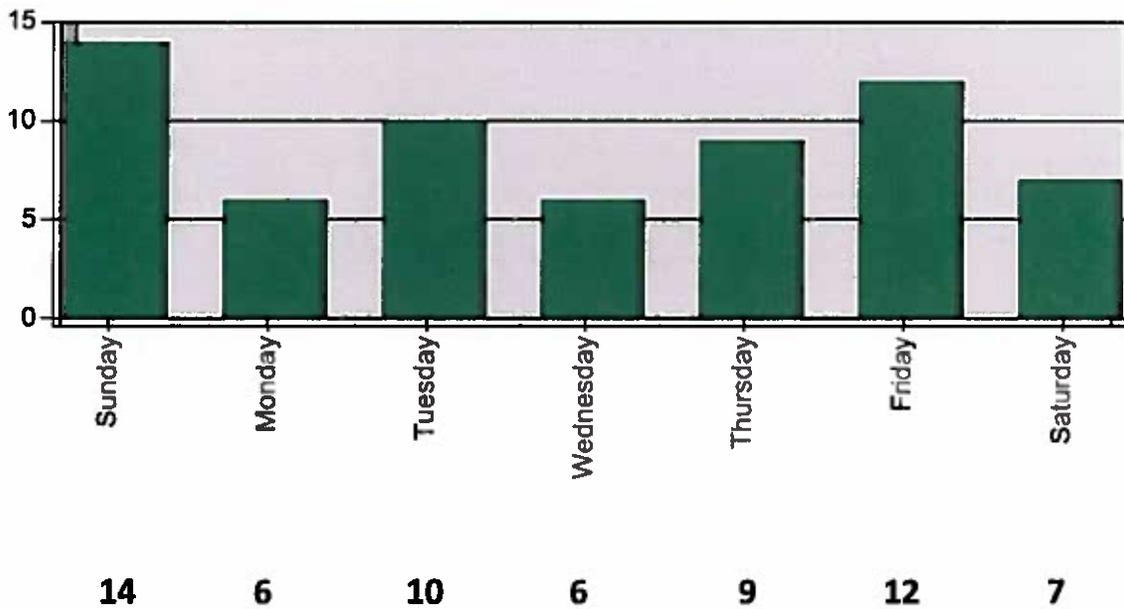
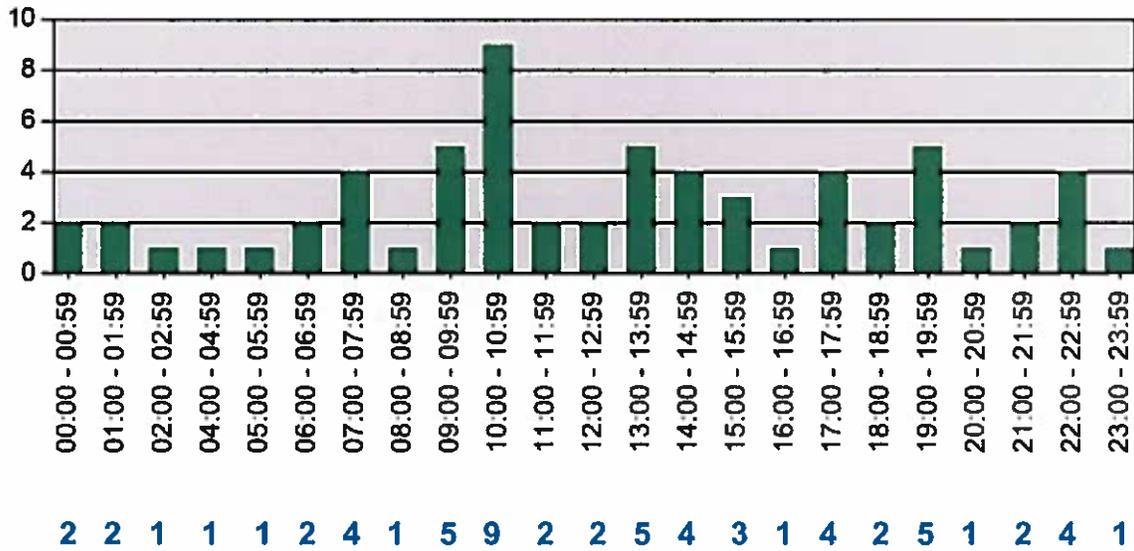
MAJOR INCIDENT TYPE # INCIDENTS % of TOTAL

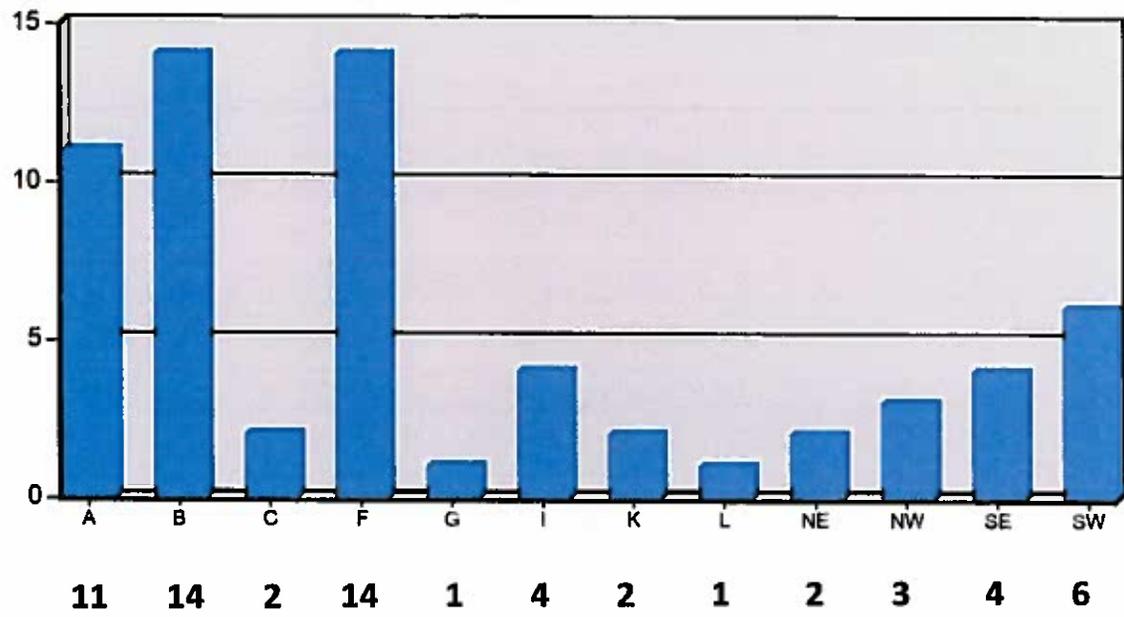
Fires	3	4.69%
Rescue & Emergency Medical Service	53	82.81%
Service Call	1	1.56%
Good Intent Call	5	7.81%
False Alarm & False Call	1	1.56%
Special Incident Type	1	1.56%
TOTAL	64	100.00%

ACTION TAKEN # INCIDENTS PERCENTAGE

00 - Action taken, other	1	1.56%
10 - Fire control or extinguishment, other	2	3.13%
11 - Extinguishment by fire service personnel	2	3.13%
21 - Search	2	3.13%
30 - Emergency medical services, other	4	6.25%
31 - Provide first aid & check for injuries	5	7.81%
32 - Provide basic life support (BLS)	22	34.38%
33 - Provide advanced life support (ALS)	16	25%
71 - Assist physically disabled	1	1.56%
82 - Notify other agencies.	2	3.13%
86 - Investigate	2	3.13%
92 - Standby	4	6.25%
93 - Cancelled en route	3	4.69%
TOTAL:	66	

Incident	Date	Type	Property loss	Content Loss	Total loss
0508	09/03/2015	160 - Special outside fire,	\$10,000.00	0	\$10,000.00





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**City of Holtville
REPORT TO COUNCIL**

MEETING DATE:	10-26-15
APPROVED FOR AGENDA	
CITY MANAGER	
FINANCE MANAGER	_____
CITY ATTORNEY	_____

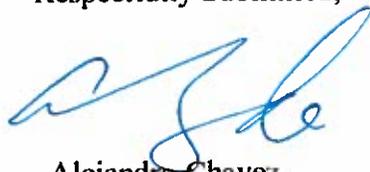
DATE ISSUED October 21, 2015
FROM: Public Works Foreman
SUBJECT: Bi Monthly Report.

THIS IS INFORMATION PROVIDED TO THE CITY COUNCIL. NO ACTION IS REQUIRED OF THE CITY COUNCIL.

The purpose of this report is to inform Council of Public Works activities since the last council meeting. Public Works has been actively working on or completed the following:

- Cleared sewer plugs at different locations in town.
- Repaired water service line leaks.
- Worked with the Sheriff's Department to clean up graffiti at park and around town.
- Patch potholes in different locations in town.
- Ran main sewer lines in different locations in town.
- Caught 1 dogs and 3cat.
- Cleaned up waterbreak mud on Brentwood Ave.
- Installed 120' of 1" irrigation line and backflow for the trees on the corner of 6th St. and Pine Ave.
- Repaired yield sign on the corner of 4th and Orchard Rd.
- Boarded up and replaced broken door at City Hall.
- Installed new 3" meter at the water station.
- Assisted with road closure for the veteran convoy.

Respectfully Submitted,



Alejandro Chavez
Public Works Foreman
City of Holtville



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City of Holtville

REPORT TO CITY COUNCIL

MEETING DATE:	10-26-15
APPROVED FOR AGENDA	
CITY MANAGER	
FINANCE MANAGER	
CITY ATTORNEY	

DATE ISSUED: October 20th, 2015

FROM: Frank Cornejo
Water/Wastewater Operations Supervisor

SUBJECT: Water / Wastewater Plant Operations & Maintenance Summary

THIS IS INFORMATION PROVIDED TO THE CITY COUNCIL. NO ACTION IS REQUIRED OF THE CITY COUNCIL.

The purpose of this report is to inform council of all operations, maintenance, and construction activities completed at the City's water-wastewater treatment facilities during the period between 10/07/15 to 10/20/15.

Also attached to this report are photos of the current wastewater treatment plant improvements construction work, as well as an updated spreadsheet detailing both the 2015 year-to-date violations related to NPDES Permit (*ammonia*) noncompliance for our wastewater facility.

Water Treatment Plant:

- Staff completed installation of new flash mix pump discharge piping at raw water ponds chemical injection station.
- Staff took all routine samples, completed all regulatory reports, and maintained operational records as required.

Waste Water Treatment Plant:

- WWTP/PW staff completed drying beds 2 and 3 dried sludge removal operations. (see photo)
- Staff took all routine samples, completed all regulatory reports, and maintained operational records as required.

Waste Water Treatment Construction:

- **Pacific Hydro work crews completed dewatering and earth moving operations for clarifier basin.**
(see attached photo)
- **Pacific Hydro work crews installed filter fabric and backfilled clarifier basin area with 6' of Class 2 base material.**
- **Pacific Hydro work crews began installing concrete form assemblies for clarifier basin structure.**

Respectfully Submitted,



Frank Cornejo.
Water/Wastewater Operations Supervisor
City of Holtville



CALIFORNIA ENVIRONMENTAL PROTECTION AGENCY
STATE WATER RESOURCES CONTROL BOARD

California Integrated Water Quality System Project (CIWQS)

Mandatory Minimum Penalty Report

SEARCH CRITERIA: [REFINE SEARCH](#) [NEW SEARCH](#) [GLOSSARY](#)
 Region (7), County (Imperial) Occurred between 1/1/2015 and 12/31/2015

Region	Agency	Facility	WDID	MMP Exempt Violations	Serious Reporting Violations	Serious Effluent Violations	Chronic Violations	Vio w/ MMP Enf	Vio Fully Resolved (Paid and Historical)	Vio w/o MMP Enf	Total MMP Vios
7	Calexico City	Calexico City WWTP	7A130101011 0	0	0	0	1	0	0	1	1
7	Callipatria City	Callipatria City WWTP	7A130102041 0	0	0	1	1	0	0	2	2
7	Holtville City	Holtville City WWTP	7A130105011 0	0	0	0	29	0	0	29	29
7	Imperial City	Imperial City WWTP	7A130106011 0	0	0	0	1	0	0	1	1
7	Imperial ID	Imperial ID El Centro GS	7A130128003 0	0	0	0	6	0	0	6	6
7	Imperial ID	Imperial ID Grass Carp Hatchery	7A130128015 0	0	0	0	13	0	0	13	13
7	Ormand, Peter M	Peter M Ormand Date Gardens MHP	7A131057011 0	0	6	0	0	5	0	1	6
7	Ralph Beatty	Country Life MHPRV Asset Partners LP WWT	7A131001011 0	0	0	0	1	0	0	1	1
7	Seeley Cnty WD	Seeley CWD WWTP	7A130111013 0	0	0	0	7	0	0	7	7
TOTAL (PAGE)				0	6	1	59	5	0	61	66
TOTAL (REPORT)				0	6	1	59	5	0	61	66

Violation: An instance of non-compliance

Serious Reporting Violation: Defined by California Water Code section 13385.1 as a failure to file a discharge monitoring report pursuant to Section 13383 for each period of 30 days following the deadline for submitting the report, if the report is designed to ensure compliance with limitations contained in waste discharge requirements that contain effluent limitations.

Serious Effluent Violation: Serious violation as defined by California Water Code section 13385(h). Waste discharge exceeds the effluent limitation for a Group I pollutant by 40% or more (CAT1), or a Group II pollutant by 20% or more (CATs).

Chronic Violation: Chronic violation as defined by California Water Code section 13385(i). To be counted as a chronic violation, there must be 3 preceding violations within a 180 day period. The fourth non-serious violation that occurs within the 180 day period is an MMP violation.

